



CALL-IN SUB COMMITTEE

TUESDAY 11 JANUARY 2005

7.30 PM

SUB-COMMITTEE AGENDA (SCRUTINY)

**COMMITTEE ROOM 5,
HARROW CIVIC CENTRE**

MEMBERSHIP (Quorum 3)

Chair: Councillor Mitzi Green

Councillors:

**Gate Jean Lammiman
Thammaiah Osborn**

Reserve Members:

- | | |
|---------------|---------------|
| 1. Blann | 1. Seymour |
| 2. Ann Groves | 2. Versallion |
| 3. Mrs R Shah | 3. Romain |

**Issued by the Legal Services Section,
Legal Services Division**

**Contact: Claire Vincent, Committee Secretary
Tel: 020 8424 1637 E-mail: claire.vincent@harrow.gov.uk**

***NOTE FOR THOSE ATTENDING THE MEETING:*
*IF YOU WISH TO DISPOSE OF THIS AGENDA, PLEASE LEAVE IT BEHIND AFTER THE MEETING.
IT WILL BE COLLECTED FOR RECYCLING.***

HARROW COUNCIL

CALL-IN SUB COMMITTEE

TUESDAY 11 JANUARY 2005

Protocol for the Operation of the Call-in Sub-Committee (Pages 1 - 2)

Protocol for Handling Portfolio Holder Decisions Referred Back by the Call-in Sub-Committee (Pages 3 - 4)

AGENDA - PART I

1. **Appointment of Chair:**
To note the appointment at the Overview and Scrutiny Committee meeting held on 24 November 2004 of Councillor Mitzi Green as Chair of the Sub-Committee for the remainder of the 2004/05 Municipal Year.
2. **Attendance by Reserve Members:**
To note the attendance at this meeting of any duly appointed Reserve Members.

Reserve Members may attend meetings:-
 - (i) to take the place of an ordinary Member for whom they are a reserve;
 - (ii) where the ordinary Member will be absent for the whole of the meeting; and
 - (iii) after notifying the Chair at the start of the meeting.
3. **Declarations of Interest:**
To receive declarations of personal or prejudicial interests, arising from business to be transacted at this meeting, from all Members present.
4. **Arrangement of Agenda:**
To consider whether any of the items listed on the agenda should be considered with the press and public excluded on the grounds that it is thought likely, in view of the nature of the business to be transacted, that there would be disclosure of confidential information in breach of an obligation of confidence or of exempt information as defined in the Local Government (Access to Information) Act 1985.
5. **Appointment of Vice-Chair:**
To appoint a Vice-Chair of the Sub-Committee for the remainder of the 2004/05 Municipal Year.
6. **Minutes:**
That the minutes of the meeting held on 3 November 2004, having been circulated, be taken as read and signed as a correct record.

7. **Call-in of Environment and Transport Portfolio Holder Decision: Petts Hill Bridge - Scheme Design and Consultation Results:**

- Enc. (a) Notice Invoking the Call-in (Pages 5 - 6)
- Enc. (b) Record of the Decision of the Environment and Transport Portfolio Holder (Pages 7 - 8)
- Enc. (c) Documentation sent to the Environment and Transport Portfolio Holder (Pages 9 - 56)

8. **Call-in of Environment and Transport Portfolio Holder Decision: Stanmore CPZ - Consultation Results:**

- Enc. (a) Notice Invoking the Call-in (Pages 57 - 58)
- Enc. (b) Record of the Decision of the Environment and Transport Portfolio Holder (Pages 59 - 62)
- Enc. (c) Documentation sent to the Environment and Transport Portfolio Holder (Pages 63 - 180)

9. **Any Other Business:**

Which the Chair has decided is urgent and cannot otherwise be dealt with.

AGENDA - PART II - NIL

Local Government (Access to Information) Act 1985: In accordance with the Local Government (Access to Information) Act 1985, this meeting is being called with less than 5 clear working days' notice by virtue of the special circumstances and grounds for urgency stated below:-

Special Circumstances/Grounds for Urgency: Under Overview and Scrutiny Procedure Rule 22.6, a meeting of the Call-in Sub-Committee must be held within 7 clear working days of the receipt of a request for call-in. This meeting therefore had to be arranged at short notice and it was not possible for the agenda to be published 5 clear working days prior to the meeting.

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1. Call-in is the process whereby a decision of the Executive, Portfolio Holder or Officer (where the latter is taking a Key Decision) taken but not implemented, may be examined by the Overview and Scrutiny Committee prior to implementation. The Overview and Scrutiny Committee has established the Call-in Sub-Committee to carry out this role. Overview and Scrutiny Procedure Rule 22 sets out the rules governing the call-in process.

The process for call-in

2. Any six of the Members of the Council and the co-opted members on the Lifelong Learning Scrutiny Sub-Committee can call in a decision of the Executive which has been taken but not implemented. (NB: Co-opted members of the Lifelong Learning Scrutiny Sub-Committee may only sign up to requests to call in decisions relating to education matters). Only decisions relating to Executive functions, whether delegated or not, may be called in.
3. Decisions of the Executive will not be implemented for 5 clear working days following the publication of the decision and a decision can only be called in within this period (this does not apply to urgent decisions - Overview and Scrutiny Procedure Rule 23 refers). The notice of the decision will state the date on which the decisions may be implemented if not called in.
4. Call-in must be by notification to the Borough Solicitor in writing or by fax, signed by all six Members/co-opted members requesting the call-in. A request for call-in by e-mail will require a separate e-mail from each of the six Members/co-opted members concerned. A proforma of a notice for call-in has been circulated for the use of Members and co-opted members.
5. In accordance with Overview and Scrutiny Procedure Rule 22.5, a notice by Members/co-opted members to invoke the call-in procedure must state at least one of the following grounds in support of the request for a call-in of the decision:-
 - (a) inadequate consultation with stakeholders prior to the decision;
 - (b) the absence of adequate evidence on which to base a decision;
 - (c) the decision is contrary to the policy framework, or contrary to, or not wholly in accordance with the budget framework;
 - (d) the action is not proportionate to the desired outcome;
 - (e) a potential human rights challenge;
 - (f) insufficient consideration of legal and financial advice.

The call-in notice should also provide details of the evidence to support the grounds for call-in.

6. Requests for call-in which, on investigation by the Borough Solicitor, are found to have been made without the support of the required number of Members or co-opted Members, or without specifying one of the grounds set out under Overview and Scrutiny Committee Rule 22.5, will not be referred to the Call-in Sub-Committee.

Referral to the Call-in Sub-Committee

7. Once a valid notice invoking the call-in procedure has been received, a meeting of the Call-in Sub-Committee will be arranged, in consultation with the Chair and Nominated Member(s) of the Sub-Committee, within seven clear working days of the receipt of the request for call-in. The other Members of the Sub-Committee will be notified of the need for a meeting, and the date thereof, at the earliest possible opportunity.
8. The papers to be considered by the Call-in Sub-Committee will be all those considered by the decision-taker when the decision was taken, the record of the decision and the written details of the call-in request. Where information material to the decision is known to officers and was not available to the decision taker, either because it only became known after the date of the decision or otherwise, such information should be drawn to the attention of Members of the Call-in Sub-Committee.

9. The papers will be sent to all Members and Reserve Members of the Sub-Committee, the Executive, the relevant Chief Officer, and all those who had signed up to the call-in. Relevant Ward Councillors will also be notified of the meeting if the issue in question is specific to a particular Ward or Wards. The Chair of the Sub-Committee may also request that the papers be sent to any other persons that he/she feels is appropriate.
10. Members sitting on the Call-in Sub-Committee should bring to the meeting an open mind and an impartial approach. Where a Member of the Sub-Committee is one of the Members calling in the decision, that Member should send a Reserve Member to the meeting of the Sub-Committee which considers the call-in, unless (for example because they are a co-opted member) they do not have a nominated Reserve.
11. The relevant Portfolio Holder and the relevant Chief Officer (or his/her representative) will be invited to attend the meeting to explain the reasons for the decision and to clarify any aspects associated with the issue in question.
12. The Members initiating the call-in will be invited to nominate one of their number or another Member who is not a Member of the Call-in Sub-Committee to advocate on their behalf and on behalf of others who may oppose the decision. Such a Member will be entitled to speak at the Call-in Sub-Committee on an equal footing with the Portfolio Holder and the relevant Chief Officer (or his/her representative).
13. The Chair of the Call-in Sub-Committee, in consultation with the meeting, may invite any other persons (for example, a legal adviser or other appropriate officer) to assist during the meeting as he/she feels appropriate.
14. The Chair of the Call-in Sub-Committee, in consultation with the meeting, will determine how the call-in will be dealt with. The rules on deputations and petitions shall apply as they apply to the Overview and Scrutiny Committee.
15. Having considered the call-in, the Sub-Committee may come to one of the following conclusions:-
 - (i) that the grounds for the call-in be upheld and
 - (a) in the event that it is upheld that the decision is contrary to the policy framework, or contrary to, or not wholly in accordance with the budget framework, the decision be referred to the Council. In such a case the Call-in Sub-Committee must set out the nature of its concerns for Council. The nature of such concerns would usually be expected to be significant and well proven in the context of the decision under consideration; or
 - (b) the decision be referred back to the decision taker for reconsideration. In such a case the Call-in Sub-Committee must set out the nature of its concerns for the decision taker. The nature of such concerns need only be sufficient to indicate that reconsideration is warranted, and need not necessarily indicate that the Sub-Committee believes the decision should be reversed, unless so stated by the Sub-Committee.
 - (ii) that the grounds for the call-in be rejected and the decision be implemented.

**PROTOCOL FOR HANDLING PORTFOLIO HOLDER DECISIONS REFERRED BACK BY
THE CALL-IN SUB-COMMITTEE**

- (1) This protocol applies to decisions made by individual Portfolio Holders (whether or not on the recommendation of an Advisory Panel) which are (a) the subject of call-in by the Call-in Sub-Committee of the Overview and Scrutiny Committee and (b) the Call-in Sub-Committee refers the decision back to the Portfolio Holder for reconsideration under Overview and Scrutiny Procedure Rule No 22.8(c).
- (2) In every case where the circumstances in the preceding paragraph arise, a local protocol shall apply to the effect that the Call-in Sub-Committee shall refer the matter to the Leader of the Council who will determine whether the matter should be referred to the Cabinet or to the Portfolio Holder.

Agreed by Cabinet, 17/12/02.

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1. NOTICE OF CALL-IN OF EXECUTIVE DECISION

In accordance with Overview and Scrutiny Procedure Rule 22, we, the undersigned, hereby give notice that we wish to call-in the Executive decision detailed in section 2 below:-

<u>NAME (PLEASE PRINT)</u>	<u>SIGNATURE</u>
..... JOHN NICKOLAY	..... <i>J. N. Nickolay</i>
..... MARILYN ASHTON	..... <i>M. Ashton</i>
..... JOYCE NICKOLAY	..... <i>Joyce Nickolay</i>
..... CIMS MOTÉ	..... <i>CIMS</i>
..... DON BILLSON	..... <i>Don Billson</i>
..... RAYMOND ARNOLD	..... <i>Raymond Arnold</i>

2. DETAILS OF EXECUTIVE DECISION

The details of the Executive decision are as follows:-

Decision: PWD 068/04. LETTS HILL BRIDGE - SCHEME DESIGN & CONSULTATION RESULTS

Made by: Cllr PAUL O'DEN
(Cabinet/relevant Portfolio Holder)

Published On: 20th DECEMBER 2004
(Date)

3. GROUND FOR CALL-IN

Please specify below the grounds for the call-in, in accordance with Overview and Scrutiny Procedure Rule 22.5 (the grounds on which an Executive decision may be called in are set out overleaf). Please note that the considerations of the Call-in Sub-Committee will focus on the grounds stated, and the Sub-Committee will seek evidence to support them. Please therefore also set out below details of the evidence to support the grounds for call-in, continuing on a separate sheet if necessary.

..... 22.5 (a) - INADEQUATE CONSULTATION

..... 22.5 (b) - LACK OF EVIDENCE

..... (See detailed reasons attached)

Reasons for call-in of PHD 068/04

Petts Hill Bridge – Scheme Design and Consultation Results

22.5 (a) – The consultation undertaken in September 2004 was inadequate in that it was confined to only two questions, the response to which was unlikely to be negative viz:

Q1 – Do you feel that something needs to be done about the bottleneck at Petts Hill Bridge?

Q2 – Do you feel that this scheme will help to improve the situation?

Bearing in mind that elected Members had questioned the adequacy of the proposed scheme and the potential threat to pedestrian security in the tunnels at the TARSAP meeting in December 2002, it would have been more meaningful to have included questions on those aspects in the consultation exercise. (This is borne out by the significant number of comments made on these aspects by those consulted.)

22.5 (b) – Minutes for the TARSAP meeting on 1st December 2004 when this matter was last raised (following a request from the Conservative Nominated Member) have not yet been made public. Indeed, Nominated Members have not yet seen draft minutes (as was requested at that meeting following a serious omission from minutes of the September 2004 meeting.) However, the Portfolio Holder’s decision purports to be based on the Panel’s minutes!

At the 1st December meeting TARSAP Members were glibly told that neither TfL nor Network Rail were willing to make further funds available for bridge replacement that would have enabled four traffic lanes and normal pedestrian facilities to be provided. This was the first time for two years the Panel had been formally told of the funding situation, although some Members had been informed privately during the intervening period by the Acting Director of Environmental Services that modifications were being made to the walls of the pedestrian tunnels to enable eventual bridge replacement to take place. The Panel was not provided with details of what negotiations (if any) had taken place between LBs of Harrow/Ealing and TfL/NetworkRail/Mayor of London, although it has subsequently come to light that a meeting of sorts took place sometime during November 2004 following a visit to the site by Mr Livingstone.

The Conservative Nominated Member wrote to the Portfolio Holder on 15th December 2004 urging that an “eleventh hour” attempt be made at Member level to persuade the funding authorities to enable bridge replacement to take place (instead of the adaptations at present intended) but has yet to receive a response.

For the reasons above it is recommended that the call-in should be upheld and further time allowed for urgent negotiations to take place at a political level before the stage is reached in March/April 2005 when preliminary works have been completed and a start has to be made on highway reconstruction.

Record of Environment and Transport Portfolio Holder's Decision
Ref: PHD 068/04

Subject:	Petts Hill Bridge – Scheme Design and Consultation Results (The report sought authority to implement the proposed scheme in order to improve traffic and pedestrian access at the Petts Hill Bridge junction).
Date of Decision:	20 December 2004
Declaration of interest (if any):	None
Key decision (Yes/No?):	No
Urgent/Non Urgent decision?:	Non-Urgent
Public/Exempt?:	Public
Options considered:	As set out in paragraph 2.2 of the published officer report and minutes of the Panel meeting.
Any other option identified by the Portfolio Holder:	None
Decision:	That (1) the comments recorded in the recommendation be noted; (2) the proposed scheme and junction layout as described in the design report in Appendix A of the officer report be approved for implementation; and (3) authority be given to officers to take all necessary steps under Section 6 of the Road Traffic Regulation Act 1984, to introduce a bus lane at the Northolt Road/Alexandra Avenue junction, subject to the consideration of any formal objections and that the details of the order making be delegated to officers.
Reasons for decision:	To enable officers to progress the scheme implementation.

Is the decision subject to call-in? YES

- YES** - *The call-in period expires on 4 January 2005 (5.00pm).
The decision can be implemented on 5 January 2005 if not called in.*
- NO** - *The decision is Urgent and can be implemented now.*

CALL-IN - this is the process whereby a decision taken by the Executive or a Portfolio Holder may be examined by the Overview and Scrutiny Committee. The Overview and Scrutiny Committee may recommend that the Executive reconsider the decision.

For further information, please contact Nick Wale on 020 8424 1323 or by e-mail: nick.wale@harrow.gov.uk

Ref: 068/04

Record of Environment and Transport Portfolio Holder's Decision Upon Receipt of Recommendation from the Traffic and Road Safety Advisory Panel meeting held on 1 December 2004

Subject:	Petts Hill Bridge – Scheme Design and Consultation Results
Status:	Part 1
Date of Decision:	
Declaration of interest by Portfolio Holder (if any):	
Key decision (Yes/No?):	No
Reasons for Urgency:	N/A
Options considered by Advisory Panel:	As set out in paragraph 2.2 of the published officer report and minutes of the Panel meeting.
Additional Options considered/identified by Portfolio Holder:	
Decision:	<p>The Portfolio Holder deferred decision for further information and or consultation/ agreed/disagreed* with the recommendations of the above named Panel which were as follows (*please delete as appropriate) :</p> <p>That (1) the comments recorded in the recommendation be noted;</p> <p>(2) the proposed scheme and junction layout as described in the design report in Appendix A of the officer report be approved for implementation; and</p> <p>(3) authority be given to officers to take all necessary steps under Section 6 of the Road Traffic Regulation Act 1984, to introduce a bus lane at the Northolt Road/Alexandra Avenue junction, subject to the consideration of any formal objections and that the details of the order making be delegated to officers.</p>
Reasons for decision:	To enable officers to progress the scheme implementation.
Additional Reasons for decision identified by Portfolio Holder (if any):	

Signature:	..... Portfolio Holder
Name:	..... (please print)
Date:	

Note: White sections of the form should be completed by the initiating department prior to receipt by the Portfolio Holder. The Portfolio Holder is requested to complete the grey sections of the form.

FOR RETURN TO NICK WALE, COMMITTEE SERVICES, ROOM 143 EXTENSION 2323
 NOTE TO PORTFOLIO HOLDER: Please note that once you have taken this decision the Authority is required, in accordance with the decision of Extraordinary Council at its meeting held on 28 May 2002 (Minute 27) to publish a record of your decision within two clear working days. In order to facilitate this, it is important that you return this document as soon as possible. Thank-you for your co-operation.

Meeting:	Traffic And Road Safety Advisory Panel
Date:	Wednesday 1 st December 2004
Subject:	Petts Hill Bridge – Scheme Design & Consultation Results
Responsible Officer:	Interim Head of Environment & Transport
Contact Officer:	Hanif Islam
Portfolio Holder:	Environment & Transport
Key Decision:	No
Status:	Part 1

Section 1: Summary

Decision Required

To recommend to the Portfolio Holder for Environment and Transport that:

1. The proposed scheme and junction layout as described in the design report in Appendix A be approved for implementation.
2. Authority be given to officers to take all necessary steps under Section 6 of the Road Traffic Regulation Act 1984, to introduce a bus lane at the Northolt Road / Alexandra Avenue junction, subject to the consideration of any formal objections and that the details of the order making be delegated to officers.

Reason for report

To enable officers to progress to scheme implementation.

Benefits

Improved bus priority at the junction of Northolt Road and Alexandra Avenue by completion of bus priority measures in the area and removal of pinch point at Petts Hill bridge.

Improvements to pedestrian and cycle routes beneath the Chiltern Line at Petts Hill, particularly segregating vehicular traffic from pedestrians and cyclists.

Enhancements to the operation of traffic signals at Northolt Road and Alexandra Avenue junction, including provision for pedestrians.

Enhancements to the urban area of the Petts Hill bridge.

Cost of Proposals

The total cost estimate at present is £4.32million to be funded by Transport for London (TfL). To date, TfL have approved the scheme and £500,000 for spending in 2004/05 and committed to a further £2.48million for 2005/06.

Risks

Construction mechanism needs to be agreed with Network Rail. Network Rail's preferred mechanism is unlikely to be acceptable in terms of cost and land acquisition requirements.

Land Agreements and track possession needs to be agreed with Network Rail.

Recent revisions to the cost estimate have meant that the total scheme cost has increased by £836,000 over the original cost estimate of £3.48million. Transport for London has not yet formally approved this increase.

The scheme requires land acquisition on both Harrow and Ealing sides. Difficulties in this area could delay the scheme.

Implications if recommendations rejected

Implementation of the scheme likely to be delayed. Committed funding from TfL would be at risk.

Section 2: Report

2.1 Brief History

The Petts Hill railway bridge carries the Chiltern Line over the road on the boundary between Harrow and Ealing, immediately south of the junction of the A312 (Northolt Road) and A4090 (Alexandra Avenue). It is well known as a 'bottleneck' for all traffic and has been identified as a source of delay to buses because of the restricted width of the carriageway under the bridge. However, remedial works constituted too big a scheme to be carried out within the limited financial and time frames set for TfL's London Bus Initiative.

Harrow has subsequently taken the lead in promoting a major improvement scheme and has obtained funding from TfL to progress a joint scheme with Ealing Council over the three years 2004/05 – 2006/07. Detailed design of the highway layout has recently been completed. Enabling works (public utilities diversions) have started on site. Actual scheme implementation is scheduled to start on site in Spring 2005 and finish in Summer 2006.

The objectives of the scheme are to achieve:

- Improved bus journey time and reliability;
- Improved pedestrian facilities;
- Improved cycle facilities;
- Local environmental improvement;
- Exploration of the potential for reducing delays to general traffic.

TfL's own consultants have produced a business case, which demonstrates significant benefits for the associated cost. The benefits include journey time savings for all traffic including buses.

Transport for London Bus Priority Team has given its support for the scheme. The London Borough of Ealing is also committed to the project.

The proposed works include:

- A new bore through the railway embankment each side of the existing bridge to provide for pedestrians and cyclists;
- Approach paths;
- Reallocation of the space released underneath the bridge to provide a third traffic lane;
- Junction modifications at Alexandra Avenue including a northbound bus lane on Northolt Road on the approach to the signals.

The proposed scheme introduces controlled pedestrian crossings at the junction of Northolt Road with Alexandra Avenue. These will enhance the safety and comfort of pedestrians crossing the junction.

In terms of traffic movement through the junction, the proposals retain the existing phasing of opposing Northolt Road – Petts Hill traffic flows running simultaneously followed by the Alexandra Avenue flows. However, the proposals introduce revised timings to assist pedestrian movement across each approach.

General traffic flow and congestion under the bridge will be improved as a result of a third traffic lane. Northbound bus flow is likely to be improved considerably, as buses will be able to utilise the less congested nearside lane to enter the short bus lane at the stop line. However, particularly in peak hours there is unlikely to be a significant improvement in traffic flow and congestion, as the increased capacity will be offset by increased time for pedestrians crossing and also by general traffic growth.

Some 8 to 10 parking spaces will be lost in the service road as a result of the carriageway widening at the Alexandra Avenue / Northolt Road junction. This loss will need to be addressed as part of the South Harrow Stage 3 Controlled Parking Zone scheduled to commence in Spring 2005.

Further details of the proposals are included in the design report in Appendix A.

Approval of the layout design is sought to enable officers to progress the scheme to implementation. The report in Appendix A contains the proposed design of the junction and explains the design approach. A copy of the plan showing the scheme requiring approval is in the report in Appendix A. A full size colour plan has been deposited in the Members' Library.

Photomontages of the proposal are included in Appendix B.

Authority is also sought to advertise draft Traffic Orders for the short bus lane adjacent to the island on Northolt Road / Alexandra Avenue junction as shown in the report in Appendix A.

A Cabinet report will shortly be prepared requesting approval to enter in to a Boundary Road Agreement with Ealing as well as authority to acquire third party land to enable the scheme to go ahead as currently designed.

2.2 Options considered

The current scheme has been promoted following advice from TfL that they would not be able to fund the preferred option, which is to install a new bridge at Petts Hill spanning over four traffic lanes. Transport for London has recently reconsidered the Council's preferred option, but although the Council has not received a formal decision on this, early indications suggest that TfL, as before, will not be prepared to fund this option due to the high costs.

2.3 Consultation

Consultation has been carried out with local residents of both Harrow and Ealing. The consultation leaflet was posted out on 3rd September 2004 mainly to postcode districts UB5 4 (Ealing) and HA2 8 (Roxeth Ward) covering approximately 11000 properties. The report in Appendix C summarises the results. The majority of respondents were in favour of the scheme. However, some key issues were raised which are presented here for ease of reference:

- **Bus Lanes:** Bus lanes are the cause of traffic congestion and should be removed - Complaints about bus lanes is a long standing issue,

particularly in the Northolt Area, and is not one that can be addressed by this project, although it does increase the extent of the bus lane north bound. The rationale of this scheme is to improve bus priority. TfL's Bus Priority Team is funding the scheme. The principle of providing bus priority and bus lanes is consistent with the Council's transport strategy.

- **Flooding under the bridge:** Resolve the flooding problem - The drainage under the bridge will be improved by the scheme but this is no guarantee that it will solve the problem that is likely to be caused by surcharging sewers into which the highway drainage discharges.
- **Traffic Issues:** There were a number of different issues relating to traffic primarily saying that congestion will increase, the scheme should address wider congestion issues, lighting phases should be altered and a filter lane for right turners into Alexandra Avenue should be installed - General traffic flow and congestion under the bridge is likely to be improved as a result of a third traffic lane. However, there is unlikely to be any significant changes in traffic flow or congestion through the Alexandra Avenue / Northolt Road junction as increased stop line capacity will be counter balanced by improvements to pedestrian crossing facilities. Northbound bus flows will be improved.
- **Safety and Security:** Residents are concerned that the underpasses will attract street crime and they should be well lit - The concerns for safety and security are well understood and the scheme has been designed in consultation with the Met Police Crime Prevention Design Team and their observations and suggestions have been taken on board including CCTV and high lighting levels.

Officers have informed consultees of the outcome of the consultation by means of a leaflet drop.

Ealing officers are currently putting a Cabinet report together to advise Members of the consultation results and to confirm Members commitment to the project.

The Harrow Public Transport Users Association has been consulted and fully supports the scheme as being a positive and worthwhile scheme to assist buses.

The scheme has received strong support from TfL.

A public meeting on the scheme was held at Welldon Park Middle School on the 18th November 2004. This was well received and gave local residents an opportunity to talk about the scheme and discuss any issues.

Representations have been received from the Chair of Danemead Grove and Petts Hill Residents Association based in Ealing objecting to the current scheme and wishing to see the implementation of the Council's 'preferred option', which is to install a new bridge at Petts Hill spanning over four traffic lanes (see also para 2.2). A copy of the letter from the residents association to the Leader of the Council is attached at Appendix D.

2.4 Financial Implications

The scheme is to be funded by TfL who have committed £2.98million for spending in years 2004/2005 and 2005/2006. Approval from TfL for the remaining £1.34million is still being sought. An announcement from TfL is expected in November 2004.

2.5 Legal Implications

Traffic Regulation Orders to provide a 'Bus Lane' as shown in the report in Appendix A can be made under Section 6 of the Road Traffic Regulation Act 1984, subject to the consideration of any formal objections.

A Boundary Road Agreement with London Borough of Ealing needs to be finalised before any works within Ealing is carried out. Land Agreement with Network Rail needs to be finalized before any work on the embankments is carried out. Details of these will be outlined in a Cabinet report.

2.6 Equalities Impact

The proposals seek to promote movement of all road users and the designs ensure that current conditions are improved upon.

Concerns have been raised about security through the pedestrian/cycle passages. These have been dealt with in the design by ensuring that these passages are short, wide and well lit with CCTV cameras and approved by the Metropolitan Police Crime Prevention Design Team.

Section 3: Supporting Information/Background Documents

Appendices

Appendix A: Scheme design report

Appendix B: Photomontages of proposed scheme

Appendix C: Consultation response report

Appendix D: Letter from Danemead Grove and Petts Hill Residents Association

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Appendix A:
Scheme design report



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enc
rough
novation

Harrow Engineering Services

Petts Hill/Alexandra Avenue



Design Report R1

November 2004

Pell Frischmann

CONSULTING ENGINEERS

Millars Three, Southmill Road, Bishop's Stortford, Herts, CM23 3DH



SECTION 1	INTRODUCTION Background The Brief Information Base Acknowledgements
SECTION 2	THE SITE Location and Description Background and Objectives Traffic Movements Road User Behaviour Accidents
SECTION 3	IMPROVEMENT PROPOSALS Early Studies Business Case Current Proposals
SECTION 4	DESIGN APPROACH Highway Layout Traffic Signals Bus Services Cycle Provision Pedestrians Heavy Goods Vehicles Road Safety Service Road Access to Dabbs Hill Property Access Lighting Signs and Markings Drainage Utilities Tunnel Structures
SECTION 5	CONSULTATION Initial Consultation Public Response

Background

- 1.1 On the boundary between the London Borough of Harrow and Ealing, Petts Hill has long been the site of long traffic queues and heavy congestion in peak periods with consequent delays to the local bus routes.
- 1.2 Improvement proposals for Petts Hill were first put forward in the Whole Route Improvement Plan (WRIP) for the Route 140 as part of the initial phase of the London Bus Initiative (LBI 1).
- 1.3 Following a number of feasibility studies it was determined to proceed with a scheme to provide three traffic lanes beneath the bridge and two tunnels through the embankments on each side to carry shared footpath-cycleways.

The Brief

- 1.4 On this basis Harrow Engineering Services on behalf of Harrow Council commissioned Pell Frischmann Consulting Engineers to develop detailed designs for the scheme, and to procure its construction.

Information Base

- 1.5 Much of the information utilised and referred to as part of the detailed design was provided by Harrow Engineering Services from the previous studies carried out in developing the scheme to this stage. A number of site visits were made for familiarisation with the existing topography, land uses and street layout and numerous photographs were taken. New traffic data was obtained via a manual classified turning count during both morning and evening peak periods. Information from Network Rail was used to develop the proposals for the footpath-cycleway tunnels.

Acknowledgements

- 1.6 The Consultants are pleased to acknowledge the valuable assistance and continuing support received from Harrow Engineering Services (Dave Masters, Vic Jenkins and Hanif Islam), the London Borough of Ealing (Peter Tonkin), Transport for London (Stuart Foster and Daniel Newcombe) and Network Rail (Bob Aldridge) as well as others from time to time.

Location and Description

- 2.1 Petts Hill is a section of the A312 principal road which links Harrow with the A40, Western Avenue. Beneath the railway bridge it also forms part of the collinear A4090 between Sudbury and North Harrow. The characteristic features of this section of road change from wide single two lane carriageway on the higher ground near the roundabout with Whitton Avenue to two narrower lanes beneath the bridge.

Immediately north of the bridge the road sweeps to the right into a long straight section of Northolt Road. At this point is a signal controlled junction with the A4090 Alexandra Avenue and there is also a separate one-way slip road along which Petts Hill traffic can reach Alexandra Avenue.

- 2.3 There is a pelican crossing to the south of the bridge and which often has queues extending back beyond the bridge in peak periods and impedes the proper operation of the signal junction causing yet more congestion.

The curving approach to the bridge means that southbound buses and large commercial vehicles overrun beyond the centre line in negotiating the bridge thus introducing potential delay, congestion and safety problems.

Background and Objectives

Following the Route 140 WRIP as part of LBI 1 Harrow Engineering Services carried out a study of the feasibility of a range of options for improving traffic conditions in the vicinity of Petts Hill. In particular this study examined ways to provide bus priority through the area to complete the missing sections of route where bus priority measures had not been introduced or facilities upgraded.

- 2.6 Two options emerged, namely, (i) a new replacement rail bridge over a widened Petts Hill carriageway allowing four traffic lanes as well as footpaths/cycleways, and (ii) widening the carriageway beneath the existing bridge to three lanes and the relocation of the footpaths into shared footway-cycleway tunnels through the embankment on each side.
- 2.7 In both options the extra carriageway width would be used to provide further bus priority.
- 2.8 The two options involved very different costs - Option 1 costing some £6-7 million became the ultimate scheme with the ideal improvements but at a high and currently unaffordable cost; Option 2 costing some £4.32 million became the interim solution offering the opportunity to progress to the ultimate scheme in time but with significant immediate benefits at a currently affordable cost.

2.9 The objectives of the scheme are:

To provide improved bus priority at the junction of Northolt Road and Alexandra Avenue by the completion of bus lane measures either side of the bridge and removal of the pinch point at Petts Hill bridge, thus increasing reliability and reducing journey time on Route 140 and other bus routes using Petts Hill.

To improve routes for pedestrians and cyclists beneath the railway at Petts Hill, particularly segregating vehicular traffic from pedestrians and cyclists. The cycle lanes will link to the existing London Cycle Network. The new route will also benefit from improved lighting and visibility.

To enhance the operation of the traffic signalled junction between Northolt Road and Alexandra Avenue, including improved provision for pedestrians.

To enhance the urban environment in the area of the Petts Hill bridge

To reduce general traffic congestion as far as possible subject to the objectives above.

Traffic Movements

2.10 Peak period traffic flows through the junction and beneath Petts Hill bridge are heavy but constrained by the congestion in the area. There are also significant flows throughout the rest of the working day. The turning movements observed in the morning and evening peak periods in July 2004 are summarised in Table 2.1 and Figure 2.1.

	Petts Hill	Alexandra Avenue	Northolt Road
Petts Hill	0	470	556
Alexandra Avenue	592	0	48
Northolt Road	694	41	0

Table 2.1A AM Peak Traffic Flows

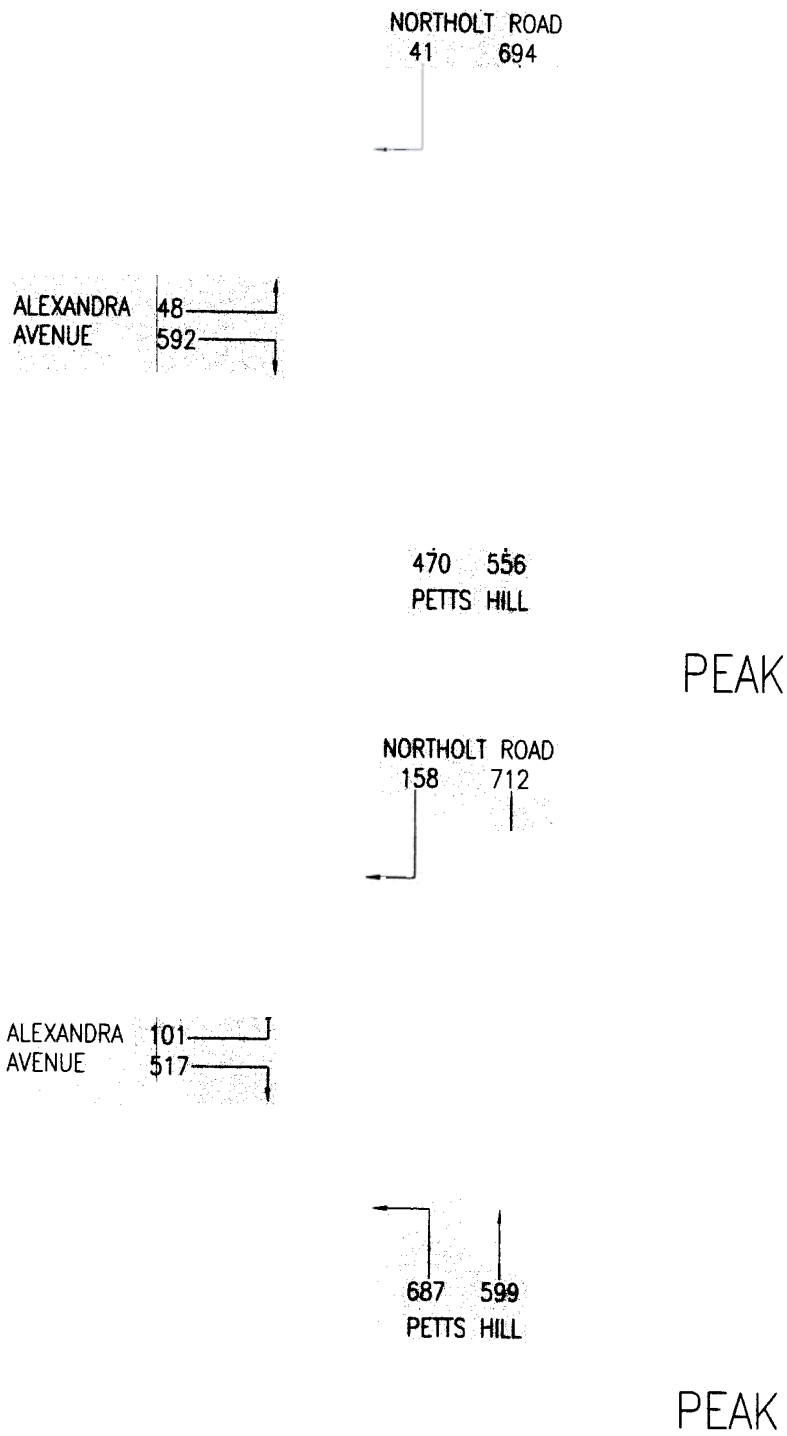


FIGURE 2.1

	Petts Hill	Alexandra Avenue	Northolt Road
Petts Hill	0	687	599
Alexandra Avenue	517	0	101
Northolt Road	712	158	0

Table 2.1B PM Peak Traffic Flows

2.11 In the morning peak there is only a modest right turning movement from Northolt Road but virtually all the Alexandra Avenue traffic turns right into Petts Hill. Some 45% of the northbound traffic on Petts Hill follows the slip road to the left onto Alexandra Avenue. In the evening peak the pattern is broadly reversed.

2.12 There are some twelve scheduled buses per hour running in each direction between Northolt Road and Petts Hill throughout the day.

Road User Behaviour

2.13 Road user behaviour is generally reasonable at this location except for traffic approaching along Alexandra Avenue in the morning peak hour. Because of the frequent delays at the pelican crossing south of the bridge this traffic frequently turns right from both right and left hand lanes of Alexandra Avenue and in funnelling into the single lane on Petts Hill typically extends back to block the forward movement of Petts Hill traffic onto Northolt Road.

Accidents

2.14 There have been a total of some 21 personal injury accidents on Petts Hill between the signal junction and the Whitton Avenue roundabout. Analysis, however, shows no unusual features except that 67% of the 9 accidents south of the bridge occurred during the hours of darkness compared with only 17% of those (12) north of the bridge. Otherwise there is the prospect of several accidents being prevented by the proposed scheme.

Early Studies

- 3.1 Following the initial study outlined above, Donaldson Associates and McNicholas were commissioned to carry out an engineering feasibility to identify the preferred method of construction of the two tunnels. Reporting in October 2001 this study examined design options, cost estimates and implementation programme paying particular attention to the requirements of Network Rail.
- 3.2 After considering six alternative construction methods the preferred option was the segmental jacked box.
- 3.3 In November 2001 H E S prepared a further feasibility study (Stage 2) considering the surveys undertaken, landtake and ownership, further Network Rail discussions and the planning and legal issues. A subsequent update of this study, in March 2003, reviewed both the new bridge and the twin tunnel option and confirm the latter as the preferred solution.
- 3.4 However amidst concern that this option might not be able to accommodate a future intermediate mode service a new option was evolved with a staged approach involving construction of the two tunnels as Stage 1 followed by their use as the abutments for a new bridge as Stage 2.

This new option with a staged construction was adopted as the preferred approach on which detailed design was to proceed.

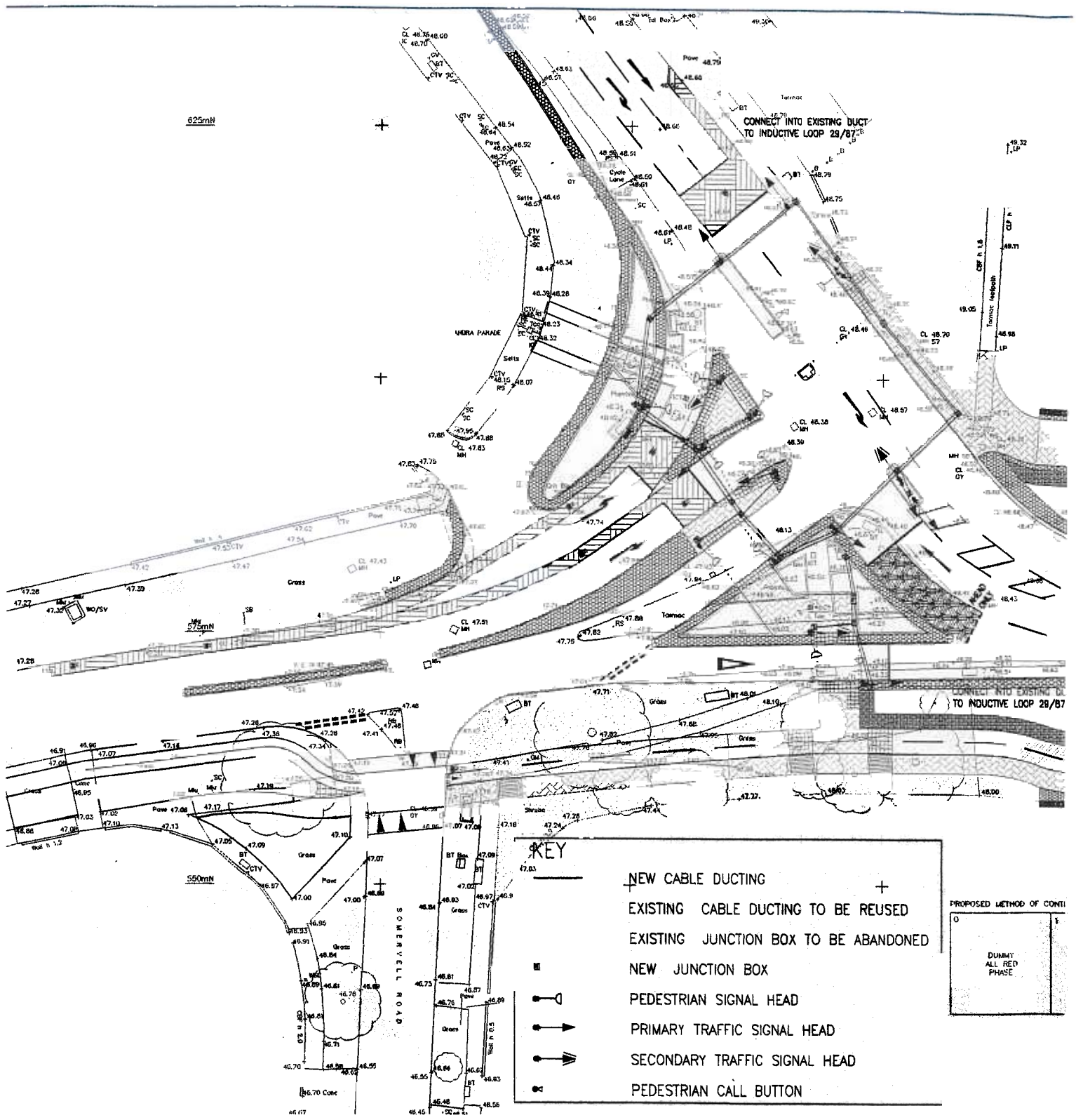
Business Case

The business case for the scheme has been reported separately, dated April 2004, and shows total discounted benefits of £9.9 million compared with costs of £3.3 million. The benefits to public transport (journey time savings and increased patronage) amount to some £1.8 million whilst some £8.0 million accrue to other traffic through time savings.

Although estimated costs emerging through the design development stage are expected to rise beyond the above figure the rise would be relatively modest and benefits would remain significantly greater than costs.

Current Proposals

The current proposals are shown on Figure 3.1.



KEY

- NEW CABLE DUCTING
- EXISTING CABLE DUCTING TO BE REUSED
- EXISTING JUNCTION BOX TO BE ABANDONED
- NEW JUNCTION BOX
- PEDESTRIAN SIGNAL HEAD
- PRIMARY TRAFFIC SIGNAL HEAD
- SECONDARY TRAFFIC SIGNAL HEAD
- PEDESTRIAN CALL BUTTON

PROPOSED METHOD OF CONTI

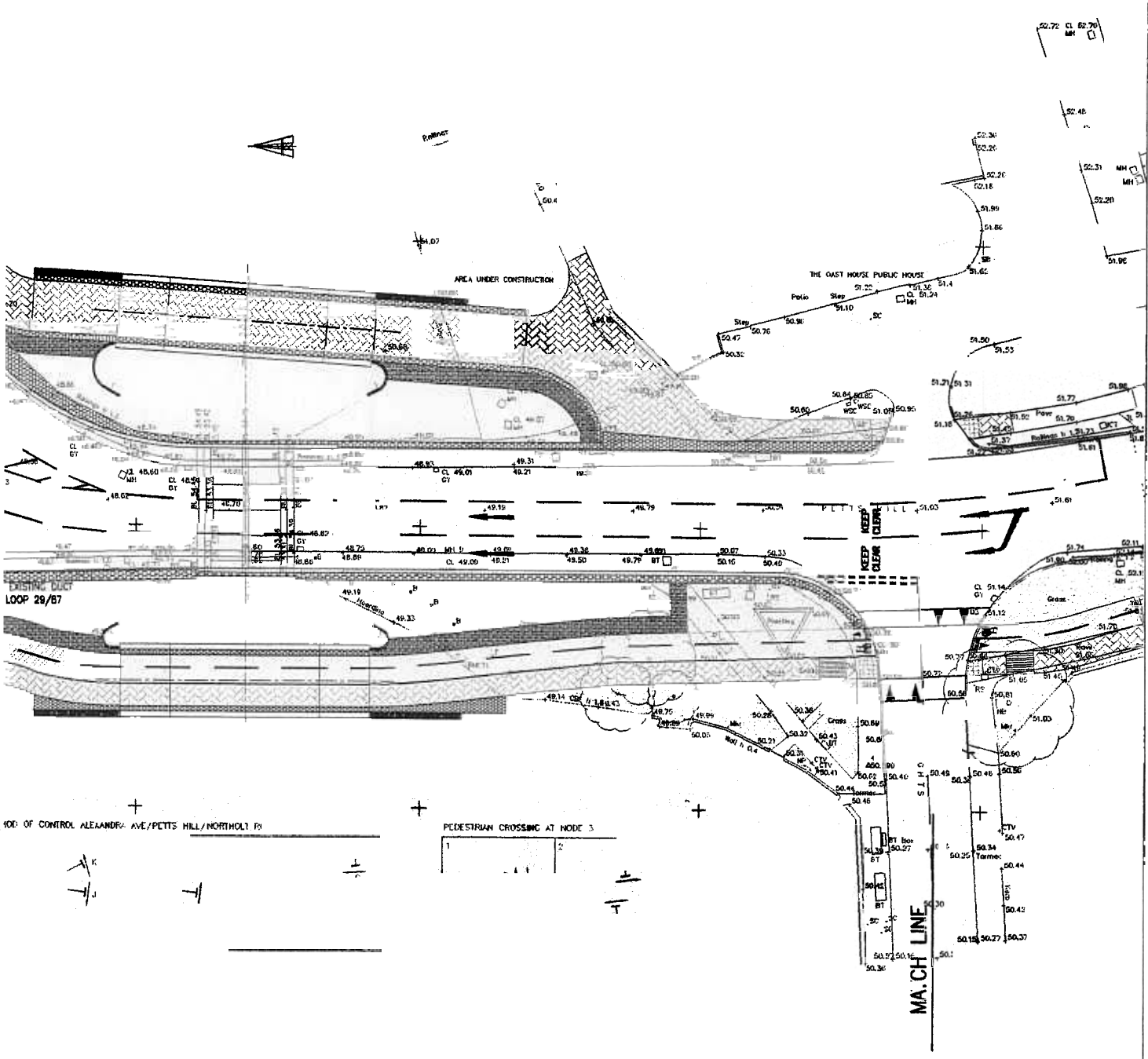
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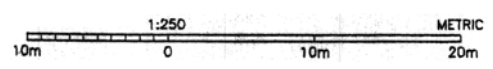
NOTES:



- DO NOT SCALE THIS DRAWING. ANY DISCREPANCIES SHALL BE NOTIFIED TO THE ENGINEER IMMEDIATELY
- ALL DIMENSIONS ARE IN MILLIMETRES - (mm)
ALL LEVELS ARE IN METRES - (m)
UNLESS NOTED OTHERWISE
- THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL PELL FRISCHMANN PROJECT DRAWINGS, DETAILS AND SPECIFICATIONS.

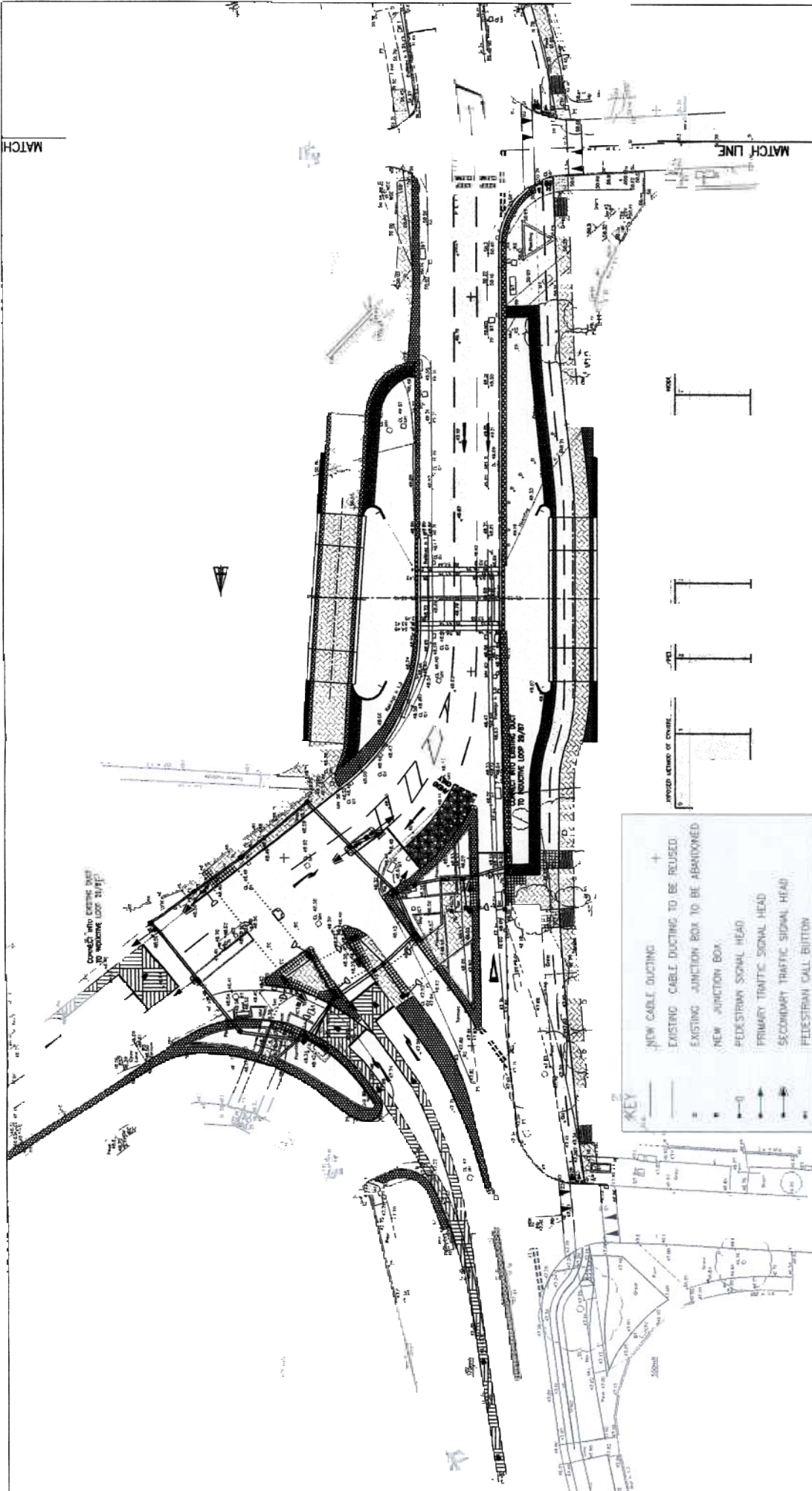
MATCH




FOR INFORMATION



 <p>HARROW ENGINEERING SERVICES</p>	<p>Client</p> <p>HARROW ENVIRONMENTAL SERVICES PROPERTY AND DEVELOPMENT</p> <p>Head of Property & Development Services GEOFF EASTON B.Sc., A.R.I.C.S. Engineering Services Manager SIMC MOTERS B.Sc., Eng., M. (Struct.), M.A.S.T. P.O. Box 39, Civic Centre, Northolt, Middlesex, PA1 3JA. Tel: 020 8863 5811 Fax: 020 8430 8611</p>	 <p>Pell Frischmann</p> <p>MILLAR THREE SOUTHWELL ROAD BISHOPS STORTFORD CH83 3DH Telephone +44 (0)1279 807 346 Facsimile +44 (0)1279 788 219 Email pfrisch@pellfrischmann.com</p>	<table border="1"> <thead> <tr> <th>Name</th> <th>Date</th> </tr> </thead> <tbody> <tr> <td>Drawn</td> <td>PS 05.10.04</td> </tr> <tr> <td>Designed</td> <td>PS 05.10.04</td> </tr> <tr> <td>Checked</td> <td>AP 05.10.04</td> </tr> <tr> <td>Approved</td> <td></td> </tr> </tbody> </table>	Name	Date	Drawn	PS 05.10.04	Designed	PS 05.10.04	Checked	AP 05.10.04	Approved	
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DATE	BY	CHKD	APP	DESC

 Peli Frischmann ENGINEERING ARCHITECTURE INTERIOR DESIGN AND LANDSCAPE ARCHITECTURE 2025 W. 34th St., Suite 200, Dallas, TX 75219 PH: 214.599.9977, FX: 214.599.9978, WWW: PELIFRISCHMANN.COM	Project No: PETTS HILL, NORTHOLT Rd, 8TH HARBOR Drawing No: JUNCTION DESIGN	Title No: Fig 3.1
	Date: 10/20/2023 Author: JW Checker: JK Designer: JK Draftsman: JK Project Manager: JK	Date: 10/20/2023 Drawn by: JW Scale: AS SHOWN

PELI FRISCHMANN ENGINEERING ARCHITECTURE INTERIOR DESIGN AND LANDSCAPE ARCHITECTURE, 2025 W. 34th St., Suite 200, Dallas, TX 75219, PH: 214.599.9977, FX: 214.599.9978, WWW: PELIFRISCHMANN.COM

Highway Layout

- 4.1 In broad terms the existing highway boundaries will remain but the scheme proposes a redistribution of the existing allocation of space together with the creation of additional space beyond the highway boundary.
- 4.2 Beneath the bridge the existing two lane carriageway and the footpath on each side will be amalgamated into a new three lane carriageway. This wider carriageway will be marked into three running lanes - the single southbound lane will be 4.5m wide and the two northbound lanes will each be 3.0m wide.
- 4.3 These 'lost' footpaths will be replaced by either footpath or shared footpath - cycleway tunnels through the railway embankment on each side of the bridge. These tunnels are expected to be owned by Network Rail but effectively operated and maintained by the local authorities as highway.
- 4.4 North of the bridge the wider carriageway will replace part of the large island at the signal junction. The two lane approach to the stopline will narrow back to a single lane beyond the junction but the merging width required to allow this will be more than a single lane and will involve a northbound relocation of the kerb and a consequent reduction in width of the service road in that area. This consequence will be exacerbated by the need to provide a minimum 1.2m width cycle lane on the southbound Northolt Road approach to the junction.
- 4.5 In Alexandra Avenue the central island has been extended from the signal junction beyond the mouth of the service road as far as Somervell Road and the left and right turn movements have been separated by a physical island.
- 4.6 To the south of the bridge the access to the JAF Vans land as well as Dabbs Hill have both been closed off in order to reduce the dangers of excessive uncontrolled access points and, in the case of the former, to remove a visibility problem.

Traffic Signals

- 4.7 The existing signals operate on a two phase system in which the opposing Northolt Road - Petts Hill traffic flows run simultaneously followed by the Alexandra Avenue flows.
- 4.8 These signals are believed to be linked to the pelican crossing to the south of the bridge. There is also a pelican crossing a similar distance to the north of the junction along Northolt Avenue but this is not thought to be linked to the junction installation.
- 4.9 There is a pelican crossing on the slip road from Petts Hill to Alexandra Avenue but there are no other signal controlled pedestrian facilities at present - crossing movements being on a 'walk with traffic' basis.

4.10 The proposed scheme will generally retain the existing phasing but will also introduce revised timings to assist pedestrian movement across each approach. The proposals provide signalised facilities for crossing each approach.

4.11 The new signal installation at the junction will be coordinated with the neighbouring pelican crossings to minimise delays.

Bus Services

4.12 The proposals include two short stretches of bus lane - between the southern pelican crossing and Danemead and on the northbound approach to the Northolt Road stopline. There are no changes to bus stops or other bus facilities.

Cycle Provision

4.13 Cycle facilities are provided extensively in the proposals and meet the requirements of both LBE and LBH Cycle Officers as well as the TfL Cycling Centre of Excellence.

4.14 The western tunnel through the embankment comprises a shared footpath-cycleway of 4.5m in width plus two 0.5m strips of deterrent paving. The shared footpath-cycleway includes a 2.5m two way cycleway, a delineator strip some 150 mm wide, and 20mm higher than the footway and cycleway level, and a footpath of some 1.85m. Both surfaces will be at the same level as indicated by TfL.

4.15 This facility will run northwards as far as Somervell Road on an alignment which offers as straight a view as possible through the tunnel as requested by the police to promote personal security. The alignment will run within the existing trees at a minimum distance of 2.0m from the boundary fence of the maisonettes numbers 1-6 Somervell Road. The alignment would match up with the existing raised crossing of Somervell Road.

4.16 South of the bridge/tunnel the alignment would again run as direct and straight as possible matching up with the pelican crossing and continuing as far as Danemead. The crossings of Dabbs Hill and The Heights would be raised to the levels of the shared facilities on each side.

4.17 Contrary to the request of the LBE Cycling Officer the crossings of side roads have left priority to traffic because both the Design Team and the Client felt strongly that to do otherwise would introduce danger from traffic stopping or slowing whilst turning from the main roads and would not pass a safety audit.

The eastern tunnel would be the same width but would be designated for pedestrians only rather than shared use because both the Design Team and the LBE Cycling Officer doubted its effectiveness in drawing cyclists off the main carriageway. Thus, southbound cyclists would remain on the highway - the kerbside lane having the greater width of 4.5m to allow buses to safely overtake cyclists.

4.19 On both Northolt Road and Alexandra Avenue approaches to the junction there will be Advanced Cycle Stoplines together with the mandatory approach lane which in both cases will be the minimum 1.2m width and relatively short in length. There will not be a similar facility on the Petts Hill approach because cyclists from that route are expected to use the shared cycleway-footway provided.

4.20 Cycle crossing facilities will be combined with those for pedestrians as a toucan on Northolt Road and well as the slip road between Petts Hill and Alexandra Avenue, although owing to a lack of space this is not practical for the remainder of Alexandra Avenue.

Pedestrians

4.21 Pedestrians will be given new facilities to cross each of the approaches to the junction under their own phase of the signals. Elsewhere they will be provided with extensive footpaths of good width and relatively straight alignment. To the south of the junction the new footways will be well clear of the highway and in places will also be separated from the highway by new cycleway.

4.22 Since the base of the tunnels will be relatively low and the level of the road rises with increasing distance south of the bridge there will be a relatively steep gradient on the footpaths especially on the eastern side of the road. To the north of the bridge however the tunnel level is close to that of the footpath and no gradient issues occur.

Heavy Goods Vehicles

The height available to vehicles beneath the bridge is signposted as 4.5m but there have been a small but regular number of incidents of vehicles hitting the abutments or overheight vehicles becoming stuck - bridge strikes.

The carriageway beneath the bridge will be reconstructed and its level will fall by up to 50 mm and thus headroom increased and strikes reduced.

Road Safety

The accident records held by the two highway authorities show some 21 personal injury accidents occurred at the site within the last three years. This number is not unduly high for such sites and the location has not figured on either authority's list of most hazardous sites.

Service Road

- 4.26** The service road alongside Northolt Road will be narrower at the curve near the access from Alexandra Avenue because of the need to widen both Northolt Road and Alexandra Avenue to the north of the signals. This will necessitate removal of the parking opportunities at the offside kerb of the service road in order to maintain the single lane for through movement.
- 4.27** The existing gap in the offside kerb allowing cycle access from Northolt Road will be closed.
- 4.28** The radius at the entry to the service road from Alexandra Avenue will be eased and a longer footpath provided. The central island on Alexandra Avenue will be extended as far as Somervell Road to prevent entry by right turning traffic which is seen as a dangerous movement.

Access to Dabbs Hill

- 4.29** Dabbs Hill, with its ornamental gates betraying its former role as access to Northolt Road Park, now has bollards across the entrance to prevent access and has long been used as a meeting point for vehicles on a range of illicit activities. Thus the London Borough of Ealing encouraged the closure of the approach and the roadside kerbs, as well as the cycleway-footpath, have therefore been continued across the former mouth of the junction.

Property Access

- 4.30** Access to existing property has been maintained throughout the design. The single exception to this is that the dropped kerb allowing access to the JAF Vans site to the south and east of the bridge has been raised to a normal kerb. This is because egress at that location does not provide adequate visibility and the present use being made of the site does not have planning permission. Access will also be difficult in any case once the scheme is complete because of the difference in level between the site and the footpath across which access would have to be taken.

Lighting

- 4.31** The lighting of the improved junction will be in accordance with Harrow Council's normal practice and will maintain and enhance the existing illumination on the street as well as introducing high quality lighting within the two tunnels for maximum personal security. The lighting has been designed and columns located so as to provide optimum illumination for both highway and cycleway-footpath.

Signs and Markings

The traffic signs and carriageway markings would follow standard practice and there are not expected to be any departures from regulations.

Drainage

The design of the drainage system for the improved scheme has made maximum use of the existing facilities and will be agreed with Thames Water.

Utilities

- 4.34** Discussions have been held with each of the utility companies and costed proposals agreed for the relocation of their equipment from the proposed carriageway and cycleway-footpath to more accessible locations. The period for their removal has been agreed as 22 weeks and would include successive closures of southbound and northbound lanes.

Tunnel Structures

The two tunnel structures, referred to as passageways, are 5.5m wide and 2.7m deep internally with an extra 1.0 metre depth chamber to carry services. The tops of the structures are located at a depth of 3.0m beneath the top of the rail as required by Network Rail.

The western tunnel (passageway) will carry a two way cycleway of 2.5m width and a footpath of 1.85m with a separator strip of 150 mm together with 0.5m wide strips of deterrent paving on each side. The eastern tunnel will be to the same dimensions but will not have a cycleway and will there be footpath only.

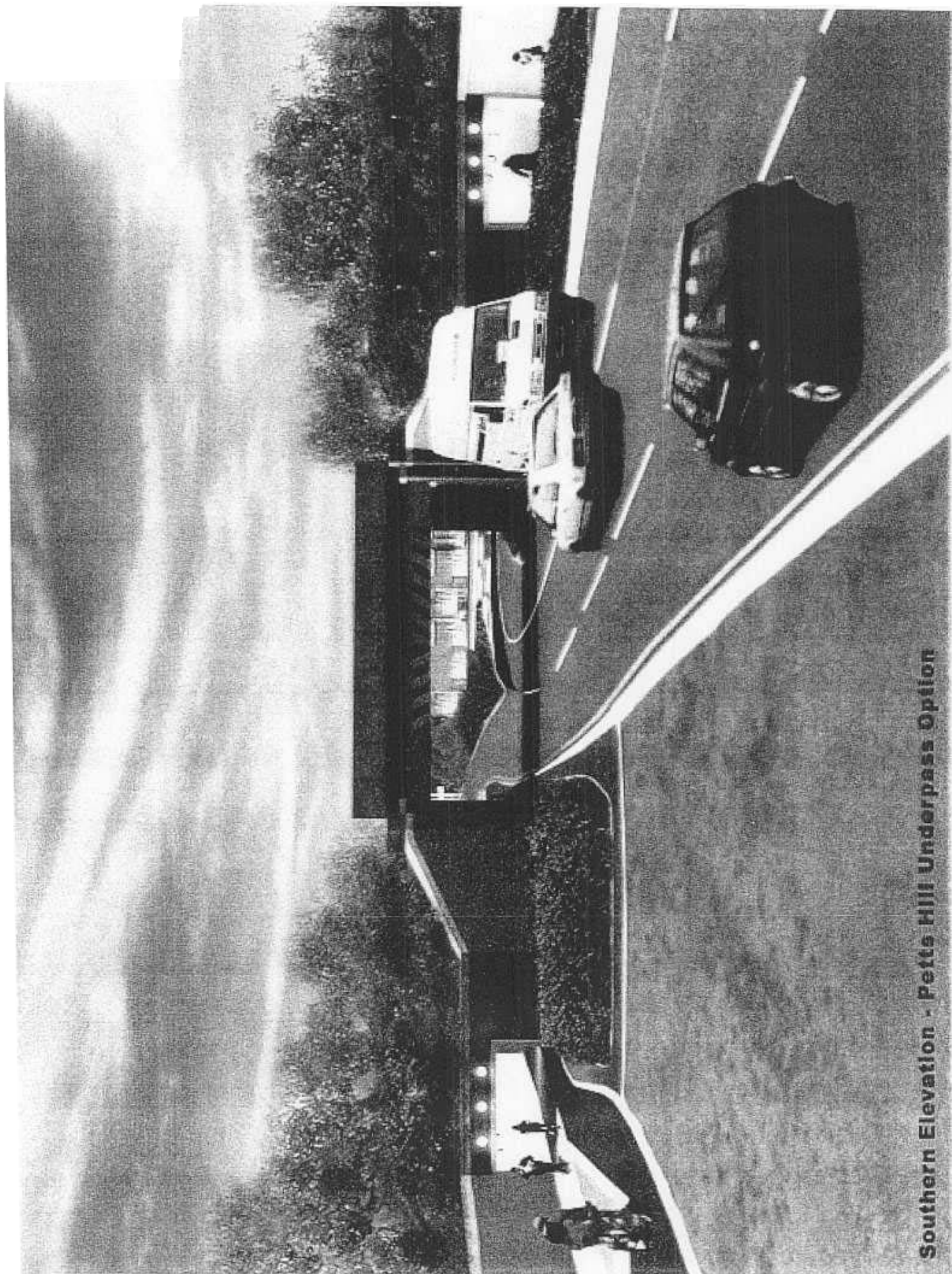
Initial Consultation

- 5.1** The initial consultation with the public about the scheme comprised the distribution of some 11,000 leaflets providing information on the problems at Petts Hill and details of the proposed improvements including coloured photomontages of the scheme.
- 5.2** Leaflets were distributed by hand to local residents and businesses and were also included in local press deliveries. Further leaflets together with plans and reports were made available in both Ealing and Harrow Civic Centres and in local libraries.

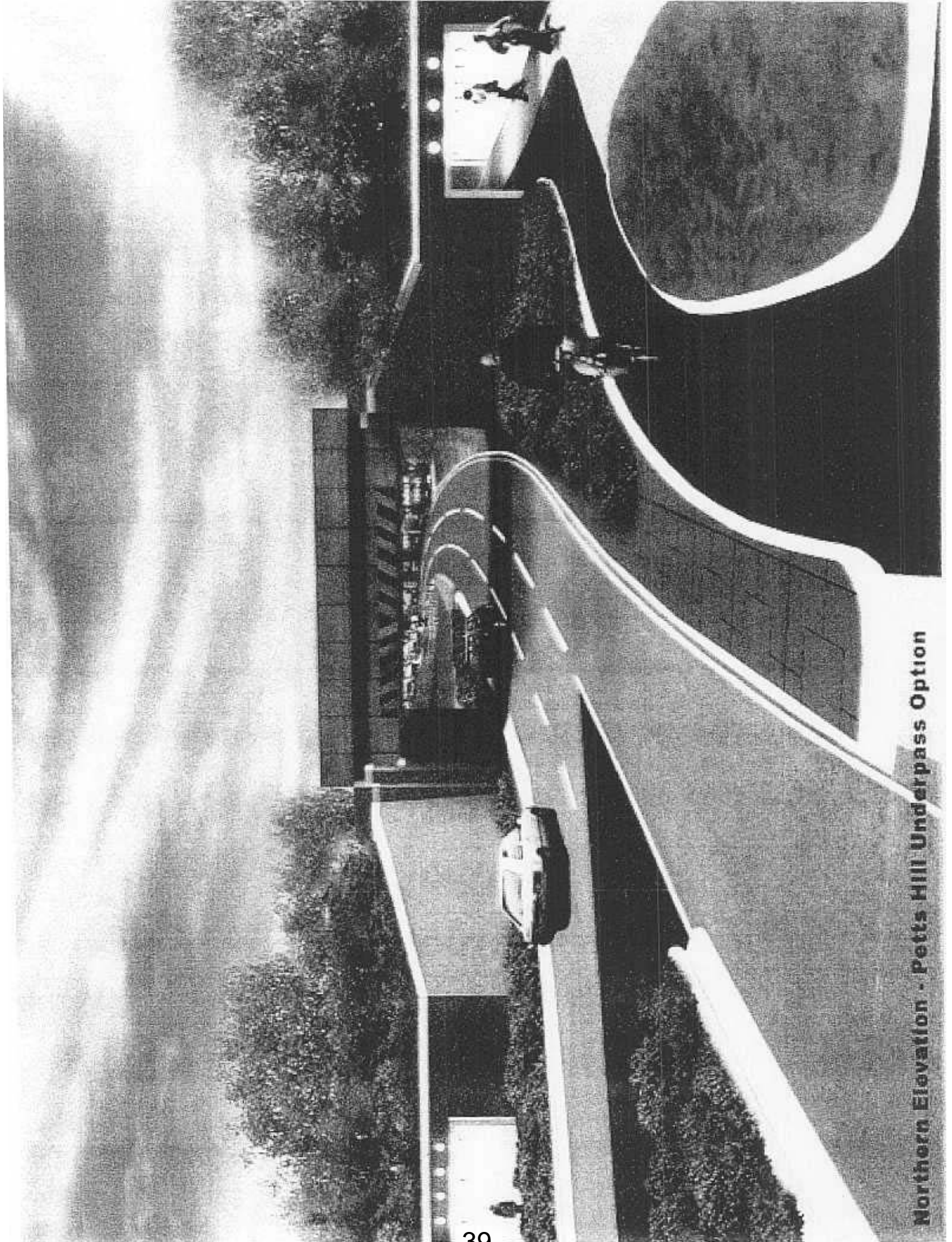
Public Response

- 5.3** The leaflets incorporated a tear-off/reply paid questionnaire as well as offering telephone enquiry services from both Councils. Whilst the number of telephone enquiries is not known there were over 400 questionnaires received representing a return of 4% on those distributed.
- 5.4** Almost threequarters of the responses received (73%) were in favour of the proposals. The main issues of objectors referred to the dissatisfaction with bus lanes, personal security and right turning facilities at the signals.

Appendix B:
Photomontages of proposed scheme



Southern Elevation - Petts Hill Underpass Option



Northern Elevation - Petts Hill Underpass Option

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Appendix C:
Consultation response report

Petts Hill Junction Improvements Report on the Responses to the Consultation Leaflet

The consultation leaflet was posted out on 3rd September 2004 mainly to postcode districts UB5 4 and HA2 8 covering approximately 11000 properties. Residents were asked to return the leaflet by 30th September responding by ticking "YES", "NO" or "DON'T KNOW" to questions:

- Q1 Do you feel that something needs to be done about the bottleneck at Petts Hill Bridge?
- Q2 Do you feel that this scheme will help to improve the situation?

Residents were also asked for any suggestions or comments about the scheme

The table in Appendix 1 summarises the responses to the tick box questions and Appendix 2 details the comments and suggestions received.

There were 451 replies equating to approximately 4.1% of the leaflets distributed, which is a low rate for response. Of the returns 73% answered Yes to both questions supporting the project whilst 8% were totally against. Don't Knows and different answers to questions 1 & 2 account for the remaining 19%. Overall it can be concluded that the majority support the project.

The comments and suggestions covered a number of areas but the most frequent messages concerned:

Bus Lanes: Bus lanes are the cause of traffic congestion and should be removed;

Flooding under the bridge: Can the flooding problem be resolved;

Traffic Issues: There were a number of different issues relating to traffic primarily saying that congestion will increase, the scheme should address wider congestion issues, lighting phases should be altered and a filter lane for right turners into Alexandra Avenue should be installed

Safety and Security: Residents are concerned that the underpasses will attract street crime and they should be well lit;

Positive Feedback: The scheme is a good idea and why has it taken so long.

Complaints about Bus Lanes is long standing issue, particularly in the Northolt Area, and is not one that can be addressed by this project, although it does increase the extent of the bus lane north bound.

The drainage under the bridge will be improved by the scheme but this is no guarantee that it will solve the problem that is likely to be caused by surcharging sewers into which the highway drainage discharges.

The phasing of the lights and the need for a filter lane are being investigated

The concerns for safety and security are well understood and the scheme has been designed in consultation with the Met Police Crime Prevention Design Team and their observations and suggestions have been taken on board including CCTV and high lighting levels.

Appendix 1

Summary of Responses to Questions 1&2

Petts Hill Questionnaire Summary of Replies

	Postal Area UB/HA	Postal Area UB	Postal Area HA	No Postcode	UB%	HA%	No Code %
Total No. Returns	451	290	137	24	64.30	30.38	5.32
Yes to question 1 & 2		204	110	16	45.23	24.44	3.55
No to question 1 & 2		28	9	0	6.23	2.00	0
Don't know to question 1 & 2		5	2	1	1.11	0.44	0.22
Don't know to question 1 No to question 2		6	2	0	1.33	0.44	0
Yes to Question 1 No to question 2		28	7	1	6.23	1.55	0.22
Yes to question 1 don't know to question 2		17	6	1	3.78	1.33	0.22
No to question 1 yes to question 2		0	0	2	0	0	0.44
No to question 1 don't know to question 2		2	0	1	0.44	0	0.22
No questions answered		0	1	0	0	0.22	0

Appendix 2

Summary of Suggestions and Comments

Petts Hill Questionnaire Replys.

Question 3.

Have you any suggestions or comments about the scheme?

The numbers in brackets () at the end of each comment shows the number of the same or similar comment or suggestion.

Bus Lane.

- Get rid of bus lane. (24)
- Bus lane is causing the traffic in the first place. (23)
- Please reduce bus lane hours. (6)
- Short bus lanes can contribute to traffic congestion. (3)
- Have bus lay-by or slip road. (3)
- Cameras should be installed to monitor bus lanes. (2)
- Shorten bus lane from Northolt Park station to Petts Hill. (2)

Construction Works.

- Minimum noise, dust and disruption to occur while works are carried out. (7)
- Appalling traffic congestion will have to be tolerated during construction. (5)
- Set up diversions for non-local traffic. (1).

Cycle Lane.

- Please remove the cycle lane. (3)
- Make sure cyclists are separated from pedestrians to maximise safety. (3)
- A barrier should be placed between cycle track and pedestrian walkway. (2)
- Anymore pavement space for cyclists would be a hazard to pedestrians. (2)

Roundabout.

- Replace lights and triangular garden with a roundabout. (7)

Flooding in underpass.

- Please deal with the flooding problem. (15)

Pedestrian Crossings.

- How will pedestrians cross the new road? Please make it easy for pedestrians to cross the road. (7)
- Please keep & or improve pelican crossing at Alexandra Avenue. (6)
- Move pelican crossing to other side of Safeway supermarket, nearer roundabout. (2)
- Put pedestrian crossings outside Netto's and Safeway supermarkets. (1)
- The bottleneck is caused by crossings either side of bridge. (1)
- I hope there are going to be plenty of slopes for mobility scooter users. (1)

Traffic.

- Congestion will increase towards South Harrow and up to roundabout. (6)
- The scheme should be widened as congestion starts at Target roundabout. (5)
- Traffic lights towards South Harrow hold up traffic flow – phasing of lights should be looked at. (5)
- Make the right turn from Northolt Road into Alexandra Avenue safer; it is currently a blind spot. Junctions from Petts Hill into Alexandra Avenue and Alexandra Avenue into Petts Hill, also need to be made safer, perhaps filter lights would help. (5)
- The traffic jams are caused by vehicles turning right into Netto supermarket and Stroud Gate, at peak times. (3)

- Worried about speeding down Alexandra Avenue due to free flowing traffic. (3)

Safety/Security.

- Worried about muggings in underpass – safety of pedestrians in underpass. No alternative for pedestrians who do not want to use underpass. (38)
- Underpass should be well lit. (17)
- Security cameras should be in underpass and vandal proof. (5)
- There should not be any ‘blind spots’ in the underpass where muggers can hide. (2)
- A real time camera link to South Harrow and Ealing police station. (2)
- Must ensure safety aspects as undesirables loiter around Oast House. (2)

Extra Lane.

- Traffic congestion will not ease unless an extra lane is put in towards Petts Hill. (4)
- It would be better to have two lanes each way. (2)
- An extra lane is not visible on plan. (1)
- Two lanes towards Heathrow would improve traffic flow. (1)

Footbridge.

- Instead of a subway a footbridge should be built. (3)
- A footbridge should be built across road to Oast House. (1)

Traffic Lights.

- Pedestrian crossing lights by service station should be moved to nearer roundabout. (2)
- Filter lights should be used at Alexandra Avenue, Petts Hill and Northolt Road junction. (1)
- Too many sets of traffic lights between Petts Hill and under bridge. (1)

Road Markings.

- Double yellow lines or red lines should be marked parallel to The Parade and opposite Netto. (3)
- Make road markings clear for left and right turns. (1)

Graffiti.

- There should be graffiti resistant surfaces in underpass. (4)
- Pedestrian tunnels attract graffiti artists. (4)

Green Verges and surroundings.

- Green verges and shrubbery beds should be kept. (5)
- Get rid of poster boards. (2)

The Heights - Entrance.

- The passage should be made easy for traffic entering and leaving The Heights. (4)
- Congestion caused by vehicles entering and leaving The Heights. (4)

Cameras.

- A camera should be placed at the junction of Alexandra Avenue and Northolt Road to catch motorists who jump the lights turning right into Petts Hill. (2)

Railway.

- The railway service should not be affected. (3)

Oast House.

- The derelict Oast Public House needs to be demolished and replaced. (6)
- What will happen to The Oast House pub? (1)

Bridge.

- Replace bridge with a wider one. (8)
- The height of the bridge should be increased to stop vehicles getting stuck underneath it. (4)

Positive feedback.

- Good/excellent idea/scheme. (29)
- Why has it taken so long to do? (10)
- Can't wait to see when it will start. A.S.A.P. (7)
- Good luck with the project. (4)
- The scheme will make a very big difference to those of us who live off Petts Hill. (4)
- I am pleased action is being taken to ease congestion in the area. (4)
- Will improve flow from South to North. (3)
- I think it will help enormously. (2)
- Appears to be a reasonable compromise. (1)
- Congratulations - South Harrow has been ignored by L.B. Harrow for too long. (1)
- Some improvement needed to this ever-growing problem for many years. (1)

Appendix D:
Letter from Danemead Grove and Petts Hill Residents Association



Danemead Grove and Petts Hill Residents Association



Chair: Mrs Linda Koupparis
51 Danemead Grove Northolt Middx. UB5 4NY
Telephone: 020 8422 3384 Mobile 0791 77 591 363
Email: lkoupparis@aol.

Dedicated to Make Northolt a Better Place for All

7th November 2004

The Leaders of the Council
London Borough of Harrow
London Borough of Ealing

Gentlemen

Petts Hill Northolt – Bridge Project.

I am writing to you on behalf of the 400 or so households represented by this Association together with a number, rising daily, in the vicinity expressing their concern of the above.

So that you are fully in the picture I will detail the location and apologise if you are already familiar with it.

Petts Hill bridge carries the Marylebone – High Wycombe railway line over the A312. From Northolt to a point under the bridge this is Petts Hill and from Harrow to a point under the bridge this is Northolt Road.

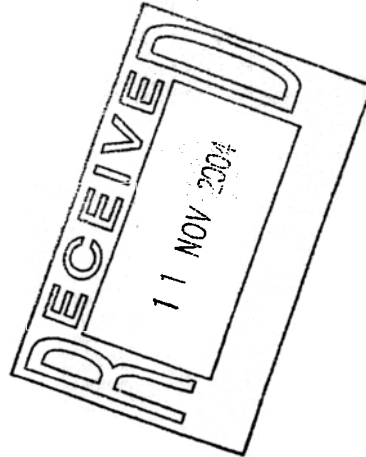
South of the bridge Petts Hill is a four lane (two in each direction) carriageway until it reaches a roundabout at which point as well as to a minor road it joins Whitton Avenue West (A4090) and Petts Hill (A312) continues towards the A40.

North of the bridge the road diverts to Alexandra Avenue (A4090) and to Northolt Road (A312) {severe bend}

Through the bridge the carriageway is of two lanes with narrow footways, protected by guardrail, on both sides.

The bridge height is indicated as 14feet 9 ins., on today's standards it should exceed 5mtrs. Additionally the condition of the bridge appears to be poor and it is extremely unsightly.

Over the years the traffic flow in both directions has increased considerably, additional bus routes have been provided, indeed the peak period bus



interchange, the provision of a supermarket in the large 24hour petrol station together with a much improved train service at Northolt Park all contribute to the situation.

It should also be noted that a fire station is located at the junction of the A312 and A4090 and on a direct route from an ambulance station located on the A4090 at Rayners Lane.

Because of the reduction in the carriageway width in the vicinity of and through the bridge the location is acknowledged as a serious bottleneck.

Having outlined the situation we would at this stage ask you to note that everyone acknowledges the need for action to be taken at this location both to resolve the traffic congestion aspects but most importantly the safety of pedestrians.

It should be noted that pedestrian traffic from the Ealing side into Harrow is of extremely high levels because of the location of schools and that South Harrow is the nearest shopping centre of any significance

On this latter aspect we would ask that you note that within the last six weeks:

A man was shot at the location.

A container carrying vehicle struck the bridge causing containers to fall.

a Child was knocked down and injured near one of the bus stops while attempting to interchange buses.

Also the murder of a man in a subway, albeit of a totally different type, elsewhere in Northolt together with high levels of violent street crime throughout the area has to be considered in the equation.

For many years various proposals to resolve the situation at the location have been announced and earlier this year a more positive announcement was made that a project was being formulated with an anticipated commencement date of spring 2005.

Details of the project are contained in the 2004/5 and 2005/6 Harrow Borough Spending Plans

The project is contained in the section relating to the bus priority programme and indeed in our view anyone unfamiliar with the area could be excused after reading the detail for believing intention of the scheme is solely to improve bus movements on the 140 route.

The proposals as we understand them are

To construct pedestrian/cyclist subways through the railway embankment on each side of the existing bridge side supports.

To remove the existing footways and re-align the carriageway so as to provide three traffic lanes through the bridge.

Associated works include the provision of CCTV, lighting and alterations to traffic signals, landscaping etc.

Within the detail proposals are outlined for phase two of the project which will entail replacing the existing bridge deck with one which is longer and supported on the pedestrian subways. There is however no indication of assurance that funding will be made available for this from TfL.

Consultation for this project has not been what we would have expected for work of this magnitude bearing in mind the effects during and after completion.

It is our view that consultation with residents should have taken place at conception and not in such a manner where we are told what we are going to have come what may.

Obviously we have had communication with Project Officers from both boroughs and TfL and indeed you may also be aware that at our request Mayor Livingstone visited the site as part of a tour of Northolt recently.

From these discussions we are concerned at statements such as:

“Ealing and Harrow would very much prefer the new bridge option and intended to bid on this basis”

From TfL

“We do not have a role monitoring the work carried out”

“But TfL is not responsible for how the borough then carries out the scheme”

With all this in mind we call upon both of you to halt this project before it starts for the following reasons:

- 1. The potential of serious crime to pedestrians through having to walk through subways.**
- 2. The ineffectiveness of one extra traffic lane solving the congestion problems.**
- 3. The potential of the risk of road traffic accidents associated with three lane traffic movements**

4. The proposed phasing of the project does not present best value from a financial point of view.
We are given to understand that proceeding to complete reconstruction as a one off will save £1.5 million of our money.
5. The proposed project is solely designed to improve bus travel times.
6. The proposed project on completion will not permit the passage of modern vehicles, on a class A road, which exceed 14ft 9ins in height. The detours required cause increased air pollution and road wear.

We feel that this would then present the perfect opportunity to undertake an in depth study of the traffic, pedestrian and bus interchange situation for the area from the roundabout in Petts Hill into Northolt Road and Alexandra Avenue.

From this, design a traffic management scheme capable of fully resolving, for now and the foreseeable future, all the issues surrounding congestion and pedestrian movements and at the same time create entrances and exits to both boroughs that they deserve and capable of dealing with today's traffic demands.

We have organised a meeting for local residents to take place on 24th November and your response by this date would be very much appreciated

For their information we have forwarded copies of this letter to our Ward Councillors and the Mayor of London.

Yours sincerely



Linda Kouparris

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TRAFFIC AND ROAD SAFETY ADVISORY PANEL

1 DECEMBER 2004

Chair: * Councillor Miles

Councillors:	* Arnold	* Ismail
	* Branch	* Kara
	* Burchell	* John Nickolay
	* Choudhury	* Anne Whitehead
	* Harriss	

* Denotes Member present

[Note: Councillors David Ashton, Marilyn Ashton, Mrs Bath, Seymour, Silver and Stephenson also attended this meeting.]

PART I - RECOMMENDATIONS**RECOMMENDATION 2 - Petts Hill Bridge - Scheme Design and Consultation Results**

Your Panel received a report of the Interim Head of Environment and Transport which detailed the improvement scheme design in relation to the Petts Hill Bridge and the results of the consultation process with local residents.

Members were informed that following the agreement to submit a bid to Transport for London (TfL), work on the improvement scheme had commenced. The Chair commended officers on their ongoing management of enquiries and the extensive consultation with local residents that had taken place.

With regard to the congestion caused by the construction work, it was advised that the Council was liaising with TfL in order to minimise the disruption experienced by motorists and officers visited the site daily in order to assess the signal times. In response, Members made several suggestions to limit the impact of the construction work such as the suspension of a number of bus lanes and the provision of extra warning notices.

Resolved to RECOMMEND: (To the Portfolio Holder for Environment and Transport)

That (1) the above comments be noted;

(2) the proposed scheme and junction layout as described in the design report in Appendix A of the officer report be approved for implementation; and



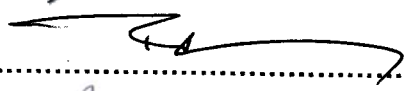
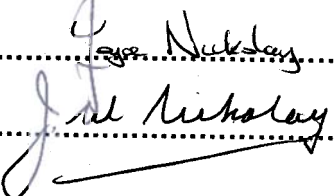
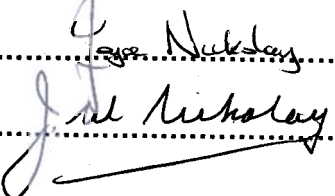
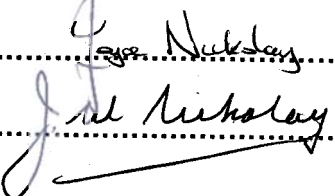
(3) authority be given to officers to take all necessary steps under Section 6 of the Road Traffic Regulation Act 1984, to introduce a bus lane at the Northolt Road/Alexandra Avenue junction, subject to the consideration of any formal objections and that the details of the order making be delegated to officers.

[REASON: To enable officers to progress to scheme implementation.]

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NOTICE OF CALL-IN OF EXECUTIVE DECISION

In accordance with Overview and Scrutiny Procedure Rule 22, we, the undersigned, hereby give notice that we wish to call-in the Executive decision detailed in section 2 below:-

<u>NAME (PLEASE PRINT)</u>	<u>SIGNATURE</u>
JANE T COWAN	
JOHN COWAN	
Richard Rowan	
RAYMOND ARNOLD	
JOHN NICKOLAY	
JOHN NICKOLAY	

2. DETAILS OF EXECUTIVE DECISION

The details of the Executive decision are as follows:-

Decision: PHD 069/04 STANMORE CR2 - CONSULTATION RESULTS

Made by: Cllr PAUL O'DELL
(Cabinet/relevant Portfolio Holder)

Published On: 20th DECEMBER 2004
(Date)

3. GROUNDINGS FOR CALL-IN

Please specify below the grounds for the call-in, in accordance with Overview and Scrutiny Procedure Rule 22.5 (the grounds on which an Executive decision may be called in are set out overleaf). Please note that the considerations of the Call-in Sub-Committee will focus on the grounds stated, and the Sub-Committee will seek evidence to support them. Please therefore also set out below details of the evidence to support the grounds for call-in, continuing on a separate sheet if necessary.

22.5 (a) - PARAGRAPH 2.3.11.11 ON PAGE 13 OF THE REPORT TO
TARBAR 1ST DECEMBER IS ALLEGED TO BE INACCURATE (see attached)

22.5 (b) - MINUTES OF TARBAR MEETING WHICH FORM BASIS OF
DECISION NOT YET PUBLISHED WAS PART OF SUCH MINUTES
SEEN BY CONSERVATIVE NOMINATED MEMBER

Reason for call-in of PHD 069/04

Stanmore CPZ – Consultation Results

Paragraph 2.3.11.11 on page 13 of the Officers' Report to TARSAP on 1st December 2004 reached the conclusion that an extension of the existing yellow line only CPZ north of Whitchurch Lane should be in the form of a Residents' Parking Zone and that this stemmed from approaches made by CAPRA. CAPRA strongly deny this and claim that the preferred option was a simple extension of the yellow line only CPZ.

Since Members of the Panel were likely to have been influenced in reaching their decision to make the extension an RPZ by the way paragraph 2.3.11.11 had been worded, an opportunity should be provided for this to be reconsidered.

In addition, statistical data resulting from the Council's consultation exercise was relatively inconclusive. At the very least residents in the roads concerned ought to be re-consulted and provided with clear details of the options under consideration.

Record of Environment and Transport Portfolio Holder's Decision
Ref: PHD 069/04

Subject:	Stanmore CPZ – Consultation Results
	(Following a review of the existing CPZ in Stanmore, the report sought authority to introduce limited extensions to the scheme. The report also proposed that further consultation be undertaken with residents of a number of roads within the planned extensions).
Date of Decision:	20 December 2004
Declaration of interest (if any):	None
Key decision (Yes/No?):	No
Urgent/Non Urgent decision?:	Non-Urgent
Public/Exempt?:	Public
Options considered:	As set out in paragraph 2.3 of the published officer report and minutes of the Panel meeting.
Any other option identified by the Portfolio Holder:	None
Decision:	<p>That (1) the existing Stanmore Town Centre Controlled Parking Zone B be extended as shown at Appendix M of the officer report;</p> <p>(2) further consultation be carried out in parallel with statutory consultation in Charlbury Avenue, Craigweil Close and Laburnum Court and if further consultation shows there is no support for inclusion in the CPZ, these roads be excluded from the scheme;</p> <p>(3) Eaton Road be re-consulted with regard to inclusion in the CPZ, in parallel with the statutory consultation;</p> <p>(4) further consultation be carried out in parallel with statutory consultation in the proposed zone H extension to include London Road (to Court Drive) and Snaresbrook Drive as shown at Appendix M of the officer report, to establish if there is support for inclusion in the proposed Monday to Saturday extension and if further consultation shows there is no support, these roads be excluded from the proposals;</p> <p>(5) a Controlled Parking Zone be created in Howberry Road between Cloyster Wood and Wychwood Avenue including Howberry Close as shown at Appendix M of the officer report to operate, Monday to</p>

Friday, 2pm – 3pm and the residents of Howberry Road and Howberry Close be written to in parallel to the statutory consultation in order to explain the benefits of the scheme;

(6) the traffic orders be amended to incorporate the on-street business permit facility for both zones;

(7) the free parking space in Merrion Avenue be converted to 18 shared use “pay and display”/residents/business spaces operating from 8am to 6.30pm, Monday to Friday as shown at Appendix K of the officer report;

(8) double yellow line waiting restrictions be introduced in Stanmore Hill at its junction with Fallowfield, Park Lane, Hilltop Way and Springfield Close as shown at Appendix O of the officer report;

(9) the existing 8am to 6.30pm yellow line waiting restrictions on the south side in Gordon Avenue at its junction with Old Church Lane be extended to the eastern wall of 7 Gordon Avenue as shown at Appendix P of the officer report;

(10) double yellow line waiting restrictions be introduced in Gordon Avenue at its junction with Water Gardens as shown at Appendix P of the officer report;

(11) the existing double yellow line waiting restrictions in Elm Park on the west side be extended northwards to a point opposite the common boundary of 4 and 6 Elm Park as shown at Appendix P of the officer report;

(12) officers be authorised to make minor amendments and finalise the detailed design in accordance with Appendices K, M, N, O and P of the officer report for order making purposes and to take all necessary steps under Sections 6, 45,46 and 49 of the Road Traffic Regulation Act 1984 to advertise the traffic orders the details of which be delegated to officers and implement the scheme subject to consideration of objections, the statement of reasons to be “to control parking”; and

(13) the head petitioners be informed accordingly.

Reasons for decision: To gain agreement for the way forward with a view to implementation of parking controls to address the Council’s stated priority of enhancing the environment and encouraging more sustainable transport activity.

Is the decision subject to call-in? YES

YES - *The call-in period expires on 4 January 2005 (5.00pm).
The decision can be implemented on 5 January 2005 if not called in.*

NO - *The decision is Urgent and can be implemented now.*

CALL-IN - this is the process whereby a decision taken by the Executive or a Portfolio Holder may be examined by the Overview and Scrutiny Committee. The Overview and Scrutiny Committee may recommend that the Executive reconsider the decision.

For further information, please contact Nick Wale on 020 8424 1323 or by e-mail: nick.wale@harrow.gov.uk

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Ref: 069/04

Record of Environment and Transport Portfolio Holder's Decision Upon Receipt of Recommendation from the Traffic and Road Safety Advisory Panel meeting held on 1 December 2004

Subject:	Stanmore CPZ – Consultation Results
Status:	Part 1
Date of Decision:	
Declaration of interest by Portfolio Holder (if any):	
Key decision (Yes/No?):	No
Reasons for Urgency:	N/A
Options considered by Advisory Panel:	As set out in paragraph 2.3 of the published officer report and the minutes of the Panel meeting.
Additional Options considered/identified by Portfolio Holder:	
Decision:	The Portfolio Holder deferred decision for further information and or consultation/ agreed/disagreed* with the recommendations of the above named Panel which were as follows (*please delete as appropriate) :

That (1) the existing Stanmore Town Centre Controlled Parking Zone B be extended as shown at Appendix M of the officer report;

(2) further consultation be carried out in parallel with statutory consultation in Charlbury Avenue, Craigweil Close and Laburnum Court and if further consultation shows there is no support for inclusion in the CPZ, these roads be excluded from the scheme;

(3) Eaton Road be re-consulted with regard to inclusion in the CPZ, in parallel with the statutory consultation;

(4) further consultation be carried out in parallel with statutory consultation in the proposed zone H extension to include London Road (to Court Drive) and Snaresbrook Drive as shown at Appendix M of the officer report, to establish if there is support for inclusion in the proposed Monday to Saturday extension and if further consultation shows there is no support, these roads be excluded from the proposals;

(5) a Controlled Parking Zone be created in Howberry Road between Cloyster Wood and Wychwood Avenue including Howberry Close as shown at Appendix M of the officer report to operate, Monday to Friday, 2pm – 3pm and the residents of Howberry Road and Howberry Close be written to in parallel to the statutory consultation in order to explain the benefits of the scheme;

(6) the traffic orders be amended to incorporate the on-street business permit facility for both zones;

(7) the free parking space in Merrion Avenue be converted to 18 shared use "pay and display"/residents/business spaces operating from 8am to 6.30pm, Monday to Friday as shown at Appendix K of the officer report;

(8) double yellow line waiting restrictions be introduced in Stanmore Hill at its junction with Fallowfield, Park Lane, Hilltop Way and Springfield Close as shown at Appendix O of the officer report;

(9) the existing 8am to 6.30pm yellow line waiting restrictions on the south side in Gordon Avenue at its junction with Old Church Lane be extended to the eastern wall of 7 Gordon Avenue as shown at Appendix P of the officer report;

(10) double yellow line waiting restrictions be introduced in Gordon Avenue at its junction with Water Gardens as shown at Appendix P of the officer report;

(11) the existing double yellow line waiting restrictions in Elm Park on the west side be extended northwards to a point opposite the common boundary of 4 and 6 Elm Park as shown at Appendix P of the officer report;

(12) officers be authorised to make minor amendments and finalise the detailed design in accordance with Appendices K, M, N, O and P of

The officer report for order making purposes and to take all necessary steps under Sections 6, 45,46 and 49 of the Road Traffic Regulation Act 1984 to advertise the traffic orders the details of which be delegated to officers and implement the scheme subject to consideration of objections, the statement of reasons to be "to control parking"; and

(13) inform the head petitioners accordingly.

Reasons for decision:

To gain agreement for the way forward with a view to implementation of parking controls to address the Council's stated priority of enhancing the environment and encouraging more sustainable transport activity.

Additional Reasons for decision identified by Portfolio Holder (if any):

Signature:	..... Portfolio Holder
Name:	..... (please print)
Date:	

Note: White sections of the form should be completed by the initiating department prior to receipt by the Portfolio Holder. The Portfolio Holder is requested to complete the grey sections of the form.

FOR RETURN TO NICK WALE, COMMITTEE SERVICES, ROOM 143 EXTENSION 2323
 NOTE TO PORTFOLIO HOLDER: Please note that once you have taken this decision the Authority is required, in accordance with the decision of Extraordinary Council at its meeting held on 28 May 2002 (Minute 27) to publish a record of your decision within two clear working days. In order to facilitate this, it is important that you return this document as soon as possible. Thank-you for your co-operation.

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Meeting:	Traffic and Road Safety Advisory Panel
Date:	1 December 2004
Subject:	Stanmore CPZ – Review of Existing CPZ’s and possible extension - Consultation Reports
Responsible Officer:	Interim Head of Environment and Transport
Contact Officer:	Steve Swain
Portfolio Holder:	Environment and Transport
Key Decision:	No
Status:	Part I

Section 1: Summary

Decision Required

- 2. Recommendations (for decision by the Environment and Transport Portfolio Holder):**
- 2.1 that the Panel recommends:**
- (a) that the existing Stanmore Town Centre Controlled Parking Zone B be extended as shown at Appendix M;**
 - (b) that further consultation be carried out in parallel with statutory consultation in Charlbury Avenue, Craigweil Close and Laburnum Court and if further consultation shows there is no support for inclusion in the CPZ, these roads be excluded from the scheme;**
 - (c) that the existing Stanmore Station Controlled Parking Zone H be extended to include London Road (to Court Drive) and Snaresbrook Drive as shown at Appendix M;**
 - (d) that further consultation be carried out in parallel with statutory consultation in the proposed Zone H extension referred to in (c) above to establish if there is support for inclusion in the proposed Monday to Saturday extension and if further consultation shows there is no support, these roads be excluded from the proposals;**
 - (e) that a Controlled Parking Zone be created in Howberry Road between Cloyster Wood and Wychwood Avenue including Howberry Close as shown at Appendix M to operate, Monday to Friday, 2pm – 3pm;**
 - (f) That the traffic orders be amended to incorporate the on-street business permit facility for both zones;**
 - (g) that the existing “pay and display” spaces in the Broadway be amended to shared use, “pay and display”/business permit holders operating from 8 am to 6.30 pm, Monday to Saturday;**
 - (h) that the free parking space in Merrion Avenue be converted to 18 shared use “pay and display”/residents/business spaces operating from 8 am to 6.30 pm, Monday to Friday as shown at Appendix K;**
 - (i) that double yellow line waiting restrictions be introduced in Stanmore Hill at its junctions with Fallowfield, Park Lane, Hilltop Way and Springfield Close as shown at Appendix O;**
 - (j) that the existing 8 am to 6.30 pm yellow line waiting restrictions on the south side in Gordon Avenue at its junction with Old Church Lane be extended to the eastern wall of 7 Gordon Avenue as shown at Appendix P;**
 - (k) that double yellow line waiting restrictions be introduced in Gordon Avenue at its junction with Water Gardens as shown at Appendix P;**
 - (l) that the existing double yellow line waiting restrictions in Elm Park on the west side be extended northwards to a point opposite the common boundary of 4 and 6 Elm Park as shown at Appendix P;**
 - (m) that officers be authorised to make minor amendments and finalise the detailed design in accordance with Appendices K, M, N, O and P for order making purposes and to take all necessary steps under Sections 6, 45, 46 and 49 of the Road Traffic Regulation Act 1984 to advertise the traffic orders the details of which be delegated to officers and implement the scheme subject to consideration of objections, the statement of reasons to be “to control parking” and**
 - (n) inform the head petitioners accordingly.**

Reason for report

To gain agreement for the way forward with a view to implementation of parking controls to address the Council's stated priority of enhancing the environment and encouraging more sustainable transport activity.

Benefits

- Responding to residents' requests.
- CPZs incorporating residents' parking schemes can improve:
 - Safety
 - Access
 - Residential amenity
- CPZs can assist management of parking in town centres to ensure more short stay shopper/visitor spaces are available.

Cost of Proposals

The estimated cost of the re-consultation and possible extension would be in the region of £70,000. Approximately £20,000 would be spent in the current financial year and the remainder in 2005-06. There are sufficient funds in this year's CPZs budget to cover this year's costs of the scheme. Next year's budget allocations are not known at this time but see paragraph 2.4

Risks

The scheme is on the Controlled Parking Zone and Residents' Parking Scheme's programme for implementation in spring 2005. It has already slipped by about six months. The likely implementation date would be autumn 2005. The impact on the CPZs programme would be minimal.

Implications if recommendations rejected

Possible dissatisfaction with the outcome of the consultation from residents in some areas, under-expenditure of allocated funding, possible knock on effect on the Controlled Parking Zones programme. Possible loss of contributory funding from developers.

Section 2: Report

2.1 Brief History

The existing Stanmore CPZs (Zones B and H) were introduced in 1994, and extended in 1996 and have remained unchanged since. Zone B has a one hour control (3pm-4pm) Monday to Friday and pay and display bays operating 8.00am – 6.30pm, Monday to Saturday in the shopping area. Zone H, around the Stanmore Station operates Monday to Saturday with a one hour control in the morning (10am – 11am) and in the afternoon (3pm – 4pm).

- 2.1.2 The Council's consultant has carried out a review of the existing Controlled Parking Zones (Zones B and H). As part of this work, the consultant has undertaken a public consultation exercise with residents/businesses within the existing zones and those within the possible extension areas. The consultation area is shown at Appendix A. The properties in a large area outside the existing zones were included in the consultation in order to inform them of the possible extension of the CPZ and to find out whether they wished the existing CPZs to be extended to include their road. The consultation area was chosen in accordance with requests from members and the local community to address a number of parking problems at various locations. Among these are the college, the area between Marsh Lane and Canons Park Station and certain streets north of the shopping area and station.
- 2.1.3 The opportunity was also taken to consult the residents/businesses in Stanmore Hill between Fallowfield and The Common including the side roads on a yellow line waiting restrictions scheme. The proposal is designed to deal with complaints concerning visibility problems caused by parked vehicles when exiting some of the properties and the side roads.
- 2.1.4 The consultation document for the existing zones is shown at Appendix B. The possible extension to Zone B document is shown at Appendix C and the possible extension to Zone H document is at Appendix D. The Stanmore Hill yellow lines consultation document is shown at Appendix E.

2.2 Options considered

See consultation.

2.3 Consultation

- 2.3.1 Ward councillors were consulted about the proposed review and possible extension of Zones B and H through two Stakeholder meetings (see minutes of two stakeholder meetings at Appendix F). All Ward Councillors were sent a copy of the consultation leaflets. Ward members have also been consulted on the outcome of the consultation and the way forward (see paragraph 2.3.12.7).
- 2.3.2 Consultation was undertaken in September 2004, with approximately 7000 leaflets being hand delivered to residents / businesses within the area shown at Appendix A.
- 2.3.4 A telephone hotline was provided by the consultants who gave residents and businesses the opportunity to discuss in detail the proposals and make observations.

2.3.5 The response rate for each individual study area is set out below: -

STUDY AREA	APPROXIMATE NO. OF LEAFLETS DELIVERED	LEAFLETS RETURNED
Review of existing Zones B and H	1486	249 (17%)
Possible Extension to Zone B	4187	1254 (30%)
Possible Extension to Zone H	1005	278 (28%)
Stanmore Hill yellow line waiting restrictions	81	48 (59%)

2.3.6 The overall response rates are considered good for this type of consultation exercise. The responses have been placed in Members' Library.

2.3.7 During the consultation period three parking roadshows were organised. Table 1 shows a list of the locations and attendance figures. A comments book for visitors was provided at the roadshows. The summary of comments is shown at Appendix G and the comments book has been placed in Members' Library.

Table 1 - Roadshows

DATE	LOCATION	TIME	APPROXIMATE ATTENDANCE
Saturday 11 September 2004	Stanmore Library	11.00am-4.00pm	300
Tuesday 14 th September 2004	Bernays Memorial Hall, Neville New Room	10.00am - 4.00pm	100
Thursday 16 th September 2004	Bernays Memorial Hall, Neville New Room	3.00pm-8.00pm	50

2.3.8 Study Area Responses

2.3.8.1 The consultation sought the views of residents / businesses about the existing CPZs (Zone B and Zone H) and also aimed to establish the extent of the perceived parking problem in the areas surrounding the two zones. Table 2 below shows how residents within the existing zones considered the existing parking controls had helped. Table 3 shows the perception of a parking problem, in the possible extension areas.

Table 2 - Ease of parking

STUDY AREA	MUCH EASIER / EASIER	NO DIFFERENT	MUCH HARDER / HARDER
Existing Zone B and Zone H	85 (34.2%)	69 (27.7%)	74 (29.8%)

Table 3 – Parking problems

STUDY AREA	NO PARKING PROBLEM	PROBLEMS PARKING
Possible Extension to Zone B	920 (73.4%)	303 (24.2%)
Possible Extension to Zone H	188 (67.6%)	83 (29.2%)

2.3.8.2 Table 4 below provides a summary of whether the residents / businesses in the existing zones felt the existing hours of control should change. Table 5 provides a summary of responses for extending parking controls within the possible extension areas.

Table 4 – Hours of operation

STUDY AREA	REMAIN THE SAME	BE LONGER	BE SHORTER
Review of existing Zone B and Zone H	144 (57.8%)	49 (19.7%)	37 (14.9%)

Table 5 – Support for extension of zones

STUDY AREA	YES	NO	DON'T KNOW
Possible Extension to Zone B	280 (22.3%)	933 (74.4%)	32 (2.6%)
Possible Extension to Zone H	78 (28.1%)	184 (66.2%)	9 (3.2%)

2.3.8.3 The majority (57.8%) of respondents have indicated that the existing hours of operation should remain the same.

2.3.8.4 In the areas that were consulted for a possible extension, the majority of respondents indicated that they did not experience parking problems. A significant number of callers on the telephone hotline, as well as at the open days, indicated that they had no parking problems and were too far away from Stanmore Town Centre and Stanmore Station to be affected.

2.3.8.5 A summary of responses is contained at Appendix H (Review of Existing Zone B and Zone H), Appendix I (Possible extension to Zone B) and Appendix J (Possible new Zone H).

2.3.9 Review of existing Zone B and Zone H

2.3.9.1 The consultation results generally revealed that residents and businesses were happy with the way that the existing zones are operating. The results

did not indicate any clear support to change the operational days or hours of the existing zones (see Appendix H).

2.3.9.2 Some roads in zone B did indicate that there was a requirement for longer hours of control (see Appendix H). Out of those who preferred different hours of control (86 responses), the largest single majority (36%) preferred to have one hour in the morning and one hour in the afternoon. A total of 17.4% indicated that they wanted 8.30am - 6.30pm restrictions while 20.9% made an alternative suggestion.

2.3.9.3 Dennis Gardens clearly indicated that they wanted longer hours of control. One hour in the morning and one hour in the afternoon is the favoured option. It would be confusing to introduce different hours of control for just one road. It is recommended that no alterations be made to the operational days and hours of the existing zones.

2.3.9.4 It is recommended that the existing “pay and display” spaces in the Broadway be amended to shared use, “pay and display”/business permit holders operating from 8 am to 6.30 pm, Monday to Saturday. It is also recommended that the free parking spaces in Merrion Avenue be converted to 18 shared use “pay and display”/business/resident permit holders operating from 8 am to 6.30 pm, Monday to Friday (see Appendix K). It is also recommended that the traffic orders be amended to incorporate the on-street business permit facility (at designated spaces) for both zones.

2.3.10 Petitions

2.3.10.1 A total of 17 petitions (see table 6) against parking controls were received during the consultation and a further two afterwards. A number of these were also submitted to Cabinet on 14 October. The general gist of the petitions is that there are no parking problems where the petitioners live and that no CPZ is necessary. The petitions are generally from residents a considerable distance away from either the boundary of the existing CPZ or from the Town Centre and where parking problems are likely to be less of a problem. An extract from each petition is at Appendix L and the full petitions have been placed in the Members’ Library. The results of the consultation are consistent with the opposition expressed in all the petitions. It is therefore recommended that these roads/areas be excluded from the proposals.

Table 6 – List of petitions

	Road(s) represented	number of signatures
1	Gordon Avenue, Sunningdale Close	44
2	Gleneagles - Gordon Avenue	27
3	Conway Close	8
4	The Chase, Conway Close, Gordon Avenue	36
5	Gordon Avenue	9
6	Green Lane, Culverlands Close	43
7	Green Lane, Cherchefelle Mews	29
8	Courtens Mews	22
9	Wolverton Rd, Savernake Court, Kenilworth House	49
10	Abercorn Road, Barn Crescent, Golf Close, Belmont Lane, Stuart Cottages, Strawberry Cottages, Aberdeen Cottages, Sunningdale Close, London Road, Stonegrove, Winscombe Way, Wetheral Drive, Thirby Cottages, Quadrangle Mews Wolverton Road, Lansdowne Road	149
11	Temple Mead Close	22
12	Colman Court - Gordon Avenue	24
13	Greyfell Close	11
14	Wychwood Close, Wildcroft Gardens, Howberry Road (between Cloyster Wood and Du Cros Drive)	118
15	Wychwood Close	28
16	Wentworth place	5
17	Peters Close	37
18	Silverston Way	55
19	Holland Close, Holland Walk	33

2.3.11 Possible extension to Zone B

- 2.3.11.1 Generally the responses received indicate that as an area there are few parking problems and that respondents are not in favour of parking controls (see Appendix I). This indicates the lack of support for the scheme in areas that are a considerable distance away from either the existing

boundary or the Town Centre where parking problems are likely to be less of a problem.

- 2.3.11.2 Most of the responses in favour of the introduction of parking controls, have come from the roads that have parking problems and are generally situated on the periphery of the existing zone. Table 7 lists the roads where the majority of respondents (55% or more) support the introduction of parking controls.

Table 7 – Roads in favour of a scheme

Aran Drive	Capuchin Close
Carr Close	Chambers Walk
Chandos Court	Culverlands Close
Dennis Lane	Goodhall Close
Halsbury Close	Hewett Close
Hill Close	Howberry Close
Lemark Close	Linden Close
Nelson Close	Rainsford Close
Sandymount Avenue	September Way
Stangate Gardens	Trenchard Close
Water Gardens	Woodside Close

- 2.3.11.3 A number of these roads are either unadopted or are in isolated areas away from the existing zone boundary or are not in an area where there is a cluster of support to be included in the extension of Zone B.
- 2.3.11.4 The majority of respondents indicated that they would like any extension to Zone B to apply Monday to Friday (see Table 8) as existing zone.

Table 8 – Days of operation

STUDY AREA	NO REPLY	MONDAY - FRIDAY	MONDAY – SATURDAY	MONDAY – SUNDAY
Which days would you like it to apply?	458 (36.6%)	639 (51.0%)	83 (6.60%)	73 (5.80%)

- 2.3.11.5 The highest single support for the operational hours was for a scheme to operate one hour a day (see Table 9). There is no clear indication as to a standard operating time among those in favour of the extension of the zone (see Appendix I).

Table 9 - Hours of operation

STUDY AREA	NO REPLY	ONE HOUR A DAY	ONE HOUR AM AND ONE HOUR PM	ONE HOUR AM OR PM AND ONE HOUR EVENING	8-30AM – 6.30PM	ALTERNATIVE SUGGESTION
Which of the following time periods would you prefer?	394 (31.4%)	511 (40.8%)	185 (14.8%)	9 (0.70%)	18 (1.4%)	91 (7.3.%)

- 2.3.11.6 Around the college, there was support for parking controls to operate longer than the existing one hour a day. Generally there was support for a scheme to operate one hour in the morning and one hour in the afternoon. However the area is too small to be considered for a sub zone operating different hours.
- 2.3.11.7 The Ridgeway respondents are split 50% for and 50% against the extension (see Appendix I). As there is no clear mandate, it is recommended that the road is not included in the proposed extension. Alternatively, The Ridgeway could be included in the traffic order with a proviso that they be re-consulted in parallel with statutory consultation. The road could then be excluded if the result of the re-consultation justified it.
- 2.3.11.8 Stanmore College is opposed to further parking controls in the area and has indicated that alterations to the existing regime is likely to have a severe effect on their operations. The college is understood to have about 300 staff and about 150 off-street spaces. This they maintain is insufficient and have requested that they should be accommodated if the zone is extended. The residents have identified the college as a source of their parking complaints. It would not be practical to improve the situation for the residents if the college is also to be accommodated. Businesses are expected to cater for their own parking needs. However, if a scheme is introduced a limited number of shared use “pay and display”/businesses spaces could be provided close to the college to assist them.
- 2.3.11.9 Green Lane respondents are also split with 20 in favour and 21 against. There are three culs-de-sac in Green Lane (Woodside Close, Ben Hale Close and Culverlands Close). All of these taken together show that there are 29 responses in favour and 26 against. Considering also the two petitions against, it is recommended that Green Lane and associated roads are not included in the scheme. Alternatively, these roads could be included in the traffic order with a proviso that they be re-consulted in parallel with statutory consultation. The roads could then be excluded if the majority view is against parking controls.

2.3.11.10 Elsewhere, the respondents indicate little overall support for an extension of the zone. However, there is support for the extension in a number of roads on the periphery of the zone. Table 10 lists the roads where there is majority support amongst the respondents. It is recommended that consideration be given to extending the zone into the roads listed in Table 10 and shown at Appendix M and detailed at Appendix N, operating Monday to Friday 3 pm – 4 pm (as existing Zone B), except for Howberry Road (between Cloyster Wood and Whychwood Avenue) and Howberry Close (see below).

2.3.11.11 The area between Cloysters Wood and Dalkieth Grove between the railway line and Marsh Lane (Canons Park Station area) was included in the consultation as a result of requests from Canons Park Residents' Association (CAPRA). There is only support for a scheme in Howberry Road (between Cloyster Wood and Whychwood Avenue) and Howberry Close (13 for and 4 against). A new zone could be created to include these residents. A yellow line waiting restrictions scheme is operating in the area south of Cloyster Wood to Whitchurch Lane. Such schemes are not appropriate as they disadvantage many residents and their visitors since no on-street parking would be permitted during the restricted hour(s). There are always some residents who rely on the on-street spaces for their parking needs. The existing yellow line waiting restrictions scheme operates Monday to Friday 2pm – 3pm. It is recommended that Howberry Road (between Cloyster Wood and Whychwood Avenue) and Howberry Close be considered for a new Controlled Parking Zone incorporating a residents' parking scheme operating Monday to Friday 2pm – 3pm (see Appendix M).

Table 10 – Proposed roads for extension

Aran Drive	Sandymount Avenue (part not already in existing CPZ)
Coverdale Close and Rainsford Close	Lemark Close
Halsbury Close	Nelson Road
Hill Close	September Way and Laurimel Close
Hewett Close	Howberry Road (Cloyster Wood to Wychwood Avenue)*
Stangate Gardens	Howberry Close*
Dennis Lane (London Road to Eaton Close)	Eton Close

* The road shown would form a new zone.

2.3.11.12 There have been no responses from Charlbury Avenue and Craigweil Close. Both of these are at the boundary of the existing zone and if the extended zone is to include the adjacent Sandymount Avenue further parking pressure is likely to be placed on these roads. Laburnum Court respondents are not in favour of parking controls, but they are also likely to be affected by displaced parking as a result of the proposed extension (see appendices M and N and M). It is therefore recommended that

Charlbury Avenue and Craigweil Close and Laburnum Court be included in the proposed extension of the zone but the residents be consulted again when the statutory order is advertised. If the majority view indicates a preference for no parking controls, they would be excluded from the zone at that stage.

2.3.12 Possible extension to Zone H

2.3.12.1 There is support in London Road and Snaresbrook Drive only. However, an overwhelming majority (70%) of those in favour have indicated a preference for a Monday to Friday scheme (see Table 11). This is different to the operational days of the existing Zone H, which is Monday to Saturday. Saturday events at Wembley stadium have been a source of complaints previously and Monday to Friday operation is likely to be problematic in this area which is close to the Station. It may also be difficult to identify a suitable location for a change in the operational days for signing purposes. It is recommended that London Road and Snaresbrook Drive properties be re-consulted reminding them of the Wembley events and seeking clarification before an informed decision about the hours of operation can be reached. This could be done in parallel with statutory consultation. It is therefore recommended that Zone H be extended to include the section of London Road (to Court Drive) not already in the zone and Snaresbrook Drive as shown at Appendix M and detailed at Appendix N. Should the results of the re-consultation still show a preference for a Monday to Friday scheme, the proposal can be downgraded accordingly.

2.3.12.2 Elsewhere, the majority of respondents (apart from Rees Drive which is unadopted) indicate that they do not have a parking problem and do not wish to see parking controls introduced.

Table 11 – Days of operation

STUDY AREA	NO REPLY	MONDAY - FRIDAY	MONDAY - SATURDAY	MONDAY - SUNDAY
Which Days would you like it to apply?	67 24.10%	151 54.30%	35 12.60%	25 9.00%

2.3.12.3 The most popular option for the operational hours was for a scheme to operate one hour a day (see Table 12), which again is different to the current operational hours of the existing Zone H. However, the London Road and Snaresbrook Drive respondents are 19 in favour of one hour operation 17 in favour of one hour am and one hour pm as existing and 4 are in favour of 8.30 am to 6.30 pm operation. Therefore, there is majority support for one hour am and one hour pm or longer operation. It follows that the existing Zone H one hour am and one hour pm operation is likely to be acceptable to the residents.

Table 12 – Hours of operation

STUDY AREA	NO REPLY	ONE HOUR A DAY	ONE HOUR AM AND ONE HOUR PM	ONE HOUR AM OR PM AND ONE HOUR EVENING	8-30AM – 6.30PM	ALTERNATIVE SUGGESTION
Which of the following time periods would you prefer?	40 14.4%	132 47.5%	65 23.4%	5 1.8%	18 6.5%	28 10.1%

2.3.12.4 Stanmore Hill between Fallowfield and The Common

2.3.12.5 The responses in table 13 below show there is no support for the proposal shown at Appendix E. The main reason for the lack of support is the loss of parking. It is therefore proposed to substantially reduce the extent of the proposed yellow line waiting restrictions as shown at Appendix O. The revised scheme would address mainly the visibility problems at the junctions with the side roads.

Table 13 –Stanmore Hill proposed yellow line waiting restrictions

Road Name	Number of Properties	Number of replies		Support proposal		Broadly support proposal		Do not support proposal	
Park Lane	24	18	75.0%	4	16.7%	2	8.3%	12	50.0%
Hilltop Way	12	6	50.0%	3	25.0%	0	0.0%	3	25.0%
Springfield Close	28	9	32.1%	1	3.6%	2	7.1%	6	21.4%
Stanmore Hill	11	12	109.1%	2	18.2%	4	36.4%	6	54.5%
The Common	6	3	50.0%	1	16.7%	0	0.0%	2	33.3%
Total	81	48	59.3%	11	13.6%	8	9.9%	29	35.8%

2.3.12.6 The opportunity has been taken to include the introduction of double yellow line waiting restrictions at the junction of Water Gardens with Gordon Avenue and other alterations to existing waiting restrictions to deal with visibility and obstructive parking complaints (see Appendix P). These proposals formed part of the detailed consultation drawings which were on display at the roadshows. Some alterations have been made in response to the comments received.

2.3.12.7 It is recommended that the proposals shown at Appendices K, L, M, N, O and P be progressed to statutory consultation as described in the foregoing and minor adjustments to the scheme be delegated to

officers. Any amendments would be carried out prior to the statutory consultation.

- 2.3.12.8 Further consultation has been carried out with Councillors David Ashton, Marilyn Ashton and Camila Bath. They have suggested that roads/areas with a majority in favour of a scheme should be considered for inclusion where the response rate has been 35% or more. The roads in favour where the response rate has been less than 35% should be considered for re-consultation. Councillor David Ashton has advised that the remaining ward members support this approach. In their view the roads in table 14 should be included in the extension of the CPZs (Zones B and H) without re-consultation and those in tables 15 and 16 be re-consulted. It follows that a decision on the way forward would need to be deferred until the re-consultation has been carried out. This would be a departure from recent practise and the recommendations of the Overview and Scrutiny Committee which advocates streamlining of the process. To speed up the procedure, in recent times, any necessary re-consultation has been carried out in parallel with the statutory consultation. Some of the roads in table 15 where there is no support have been deleted from the proposed extension and those in table 16 are to be re-consulted in parallel with traffic order advertising.
- 2.3.12.9 Separate re-consultation (not in parallel with statutory consultation) would delay implementation. The scheme is currently on the programme for implementation in spring 2005. This has already slipped to autumn 2005 and re-consultation would delay the introduction of the scheme to spring 2006.
- 2.3.12.10 Apart from the Edgware CPZ extension which is due to be implemented in January 2005, this is the only CPZ on the programme for investigation/implementation in this financial year (notwithstanding slippage). Edgware CPZ would be unaffected if a separate re-consultation is carried out. There are two schemes on the CPZs programme for consultation in 2005-06 (Harrow Town Centre Review and South Harrow Stage 3). The timetable for implementation of both schemes is 2006-07. These would also be unaffected as two separate teams are handling them. Any re-consultation will be carried out by consultants but officer time would be spent to brief and manage them and to deal with the outcome. Whilst re-consultation would not affect the CPZ programme, there would be a knock on effect on other work such as the local safety schemes and 20 mph zones programmes. The impact on these would be difficult to quantify. (See paragraph 2.4 for financial implications).

Table 14 – Zone B responses

Road name	Number of properties	Number of replies	% Return	Are you in favour of parking controls?							
				No reply		Yes		No		Don't know	
					%		%		%		%
Arran Drive	68	26	38	1	4	21	80	3	12	1	4
Coverdale Close including Rainsford Close	21	8	38	0	0	7	88	1	12	0	0
Halsbury Close	4	2	50	0	0	2	100	0	0	0	0
Hewett Close	12	5	41	0	0	4	80	0	0	1	20
Lemark Close	12	9	75	0	0	9	100	0	0	0	0
Stangate Gardens	8	3	38	0	0	3	100	0	0	0	0
Woodside Close	5	3	60	0	0	3	100	0	0	0	0
Totals	130	56	43	1	2	49	88	4	7	2	4

Table 15 – Zone B possible extension responses

Road name	Number of properties	Number of replies	% Return	Are you in favour of parking controls?							
				No reply		Yes		No		Don't know	
					%		%		%		%
Ben Hale Close	9	4	44	0	0	1	25	2	50	1	25
Culverlands Close	31	7	23	0	0	5	62	3	38	0	0
Eaton Close	47	12	26	1	8	6	50	5	42	0	0
Gordon Avenue (part)	71	35	49	0	0	5	14	30	86	0	0
Green Lane	123	46	37	1	2	20	44	21	47	3	6
Hall Farm Close	10	2	20	0	0	1	50	1	50	0	0
Hill Close	24	4	17	0	0	3	75	0	0	1	25
Howberry Close*	33	11	33	0	0	9	8	2	18	0	0
Howberry Road* (part)	75	22	29	0	0	8	36	14	64	0	0
Linden Close	7	1	14	0	0	1	100	0	0	0	0
Nelson Road	34	10	29	0	0	7	70	3	30	0	0
Sandymount Avenue (extension area)	46	12	26	0	0	9	75	3	25	0	0
September Way	146	32	22	0	0	18	56	12	38	2	6
Laurimel Close	4	4	100	0	0	2	50	2	50	0	0
The Ridgeway	44	18	40	0	0	9	50	9	50	0	0
Uxbridge Road (part)	49	7	14	1	14	2	29	3	43	1	14

Water Gardens	18	3	17	0	0	2	67	1	33	0	0
Charlbury Av	8	0	0								
Craigweil Cl	19	0	0								
Totals	819	238	29	3	1	115	48	112	47	8	3

* Howberry Road and Howberry Close will form a separate zone.

Table 16 - Zone H possible extension responses

Road name	Number of properties	Number of Replies	% return	Are you in favour of parking controls							
				No reply		Yes		No		Don't know	
					%		%		%		%
London Road	45	21	46	3	14	10	47	8	38	0	0
Snaresbrook Drive	48	26	54	0	0	13	50	11	42	2	8
Totals	93	47	51	3	6	23	49	19	40	2	4

Note: Property figures only include properties within the proposed extension

2.4 Financial Implications

2.4.1 Consultation so far has cost approximately £30,000. Transport for London (TfL) has contributed £10,000 towards the costs. The Council has secured contributory funding of £7,000 from the developer of the former Government Offices site in London Road to introduce an extension to the existing CPZ. The time limit for funding is 6 February 2006. The Council has also secured £20,000 from Sainsbury's to review the parking controls immediate vicinity of their Elm Park development. The time limit for this is 19 March 2006. Any shortfall will be covered by this financial year's CPZs' allocation of £100,000.

2.4.2 The estimated cost of a separate re-consultation (as suggested by ward members consulted) is in the region of £10,000. The recommended re-consultation of the proposed extensions in parallel is likely to cost in the region of £1500. The reason for the difference is that there would 16 more roads up for re-consultation in the former method. Additionally, separate re-consultation would also require a further report to the Executive.

2.4.3 A bid of £100,000 has been made for CPZ investigations and implementations in each of the next two financial years. It is anticipated that any monies not used in this financial year would be transferred to the next. This year's budget would be under-spent by about £50,000. Assuming this can be carried over, it could be used to fund implementation of the scheme in 2005-06. There are two schemes for consultation in the next financial year (Harrow Town Centre Review and South Harrow). Also assuming the £100,000 capital bid for CPZs

in 2005-06 is agreed, the cost of re-consultation can be absorbed without affecting other schemes on the CPZ programme.

2.5 Legal Implications

2.5.1 Controlled parking zones and associated waiting and loading restrictions can be implemented under Sections 6, 45, 46 and 49 of the Road Traffic Regulation Act 1984.

2.6 Equalities Impact

2.6.1 Not applicable.

Section 3: Supporting Information/Background Documents

Appendices:

- Appendix A - Consultation area
- Appendix B - Existing zones review consultation document
- Appendix C - Extension to Zone B Consultation document
- Appendix D - Extension to Zone H Consultation document
- Appendix E - Stanmore Hill waiting restrictions consultation document
- Appendix F - Minutes of two stakeholder meetings
- Appendix G - Summary of roadshow comments
- Appendix H - Summary of existing zones B & H responses
- Appendix I - Summary of extension to zone B responses
- Appendix J - Summary of extension to zone H responses
- Appendix K - Proposed Merrion Avenue "pay and display"/business spaces
- Appendix L - Petitions
- Appendix M - Proposed Zones B and H extension and proposed new zone
- Appendix N -Detailed plans
- Appendix O - Proposed Stanmore Hill yellow line waiting restrictions
- Appendix P - Proposed double yellow line waiting restrictions and alterations to existing waiting restrictions

Background papers: Controlled Parking Zones and Residents' Parking Schemes 2004-5 programme, consultation, petitions.

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APPENDIX A

This appendix consists of an ordnance survey map which is not available electronically.

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A review of the existing Stanmore Controlled Parking Zones (Zone B and Zone H)

App B

APPENDIX B



We want to know your views

INTRODUCTION

The purpose of this consultation is to find out how well the Stanmore Controlled Parking Zones (CPZs) (Zones B and H) have been operating and to see if any changes are necessary. It provides an opportunity for you to express to us your views on the operation and effectiveness of the current CPZs and to let us know how to make it more effective.

The Stanmore CPZs (Zones B & H) were introduced in 1994, extended in 1996 and have remained unchanged since. It has been decided to undertake a review to rationalise the parking in the existing zones and maximize the existing parking spaces.

The B zone has a one hour control (3pm-4pm) Monday to Friday and pay and display bays (8am-6.30pm) Monday to Saturday in the shopping area.

The H zone around the Stanmore Underground Station operates Monday to Saturday with a one hour control in the morning (10am-11am) and in the afternoon (3pm-4pm).

A number of options are proposed for your consideration. These have been formulated to reflect the feedback received from the local community.

For your information we are also consulting with residents outside of the two existing zones, to identify whether there is a need to extend the CPZs into their area.

HOW DO CPZs WORK?

CPZs work by ensuring that vehicles park in designated bays during the hours the CPZ operates. At other times parking is unrestricted except where yellow lines operate for longer periods. Any vehicles that are parked illegally during the controlled times are liable to receive a Penalty Charge Notice (parking ticket).

Service/delivery vehicles can load or unload for up to 20 minutes on the yellow lines where there are no loading restrictions and in residents' bays. However, someone must be in attendance at all times to avoid a parking ticket.

PARKING ROADSHOWS

Parking roadshows will be held at:

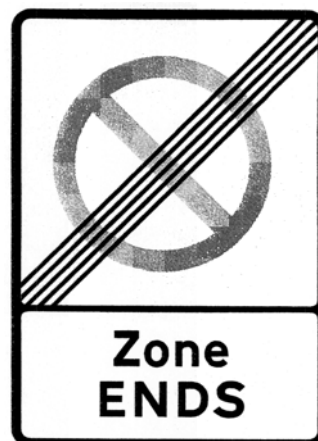
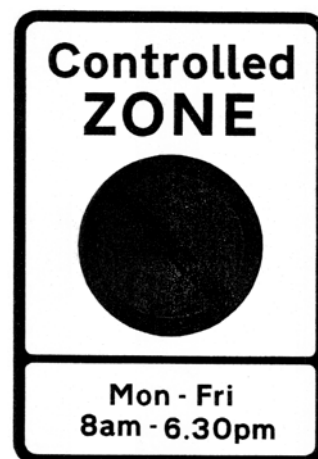
**Stanmore Library -
8 Stanmore Hill
Stanmore**

Saturday 11th September 2004 (11am to 4pm)

**Bernays Memorial Hall -
Neville New Room
25 The Broadway
Stanmore**

**Tuesday 14th September 2004 (10am to 4pm)
Thursday 16th September 2004 (3pm to 8pm)**

Project Centre staff will be on hand to answer questions on controlled parking zones and detailed plans of the possible controls will be on display. Please come along.



TYPES OF PARKING BAYS AND WHO CAN USE THEM

Residents permit parking

Bays would be marked for the use of vehicles displaying a valid parking permit during the hours of operation. Residents of the zone would be eligible for parking permits. One permit is needed for each vehicle parked on street during the hours the zone operates.

No. of permits	Cost
1st permit	£40 per year
2nd permit	£50 per year
3rd permit	£70 per year
4th and subsequent permits	£115 per year

Visitor parking

Residents' visitors could park in private driveways where this is possible or arrange their visits outside the restricted time. Alternatively, they may use "Visitor Permits" to park on street during the operational hours of the scheme. It should be noted that accommodation of visitors vehicles within the zone could reduce on street parking provision for residents themselves since they would occupy the same parking bays. Visitors can park in a resident's permit bay during the hours of control provided a valid visitor permit is displayed in their vehicle. Outside the hours of control visitors do not need to display a permit.

Visitor permits come in the form of "scratch cards" and are sold in books of 10. The current cost is detailed below (the operational hours that the proposed zone would operate will depend on the outcome of this consultation):

'all day' zone = £15 (£7.50 for senior citizens)

'one hour' zone = £10 (£5 for senior citizens)

Each visitors permit would be valid for either the morning or afternoon. Two visitor permits would be needed to park all day.

It should be noted that only residents are eligible for visitors' permits and a maximum of 10 books per household is issued per year (only two can be purchased at any one time).

Business permits

Businesses would be entitled to purchase on-street business parking permits. However, applicants must demonstrate that vehicles are necessary for the conduct of their business.

The number of permits per business will be limited to two. Provision of parking for staff is a matter for the businesses to provide as they see fit. Business permit holders will be allowed to park in specifically signed bays. The maximum stay would be limited to three hours with no return within 30 minutes.

The cost of the permit will be **£300 per permit**.

Shared use bays

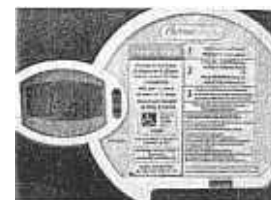
There are a number of different types of shared use bays, which incorporate the three permit groups while providing short stay parking for visitors. The type of bay will depend on the nature of the area. Residents, businesses and visitors must display a valid permit whilst parked in these bays. The maximum stay for a pay and display ticket holder is 3 hours with no return within 30 minutes.



Short stay pay and display bays

Visitors to the area will be able to park in shared use bays. They will need to purchase a pay and display ticket and the length of time they park will be restricted.

The tariff structure for the pay and display spaces will be **40p per half hour** with a maximum stay of 3 hours and no return within 30 minutes.



Parking for disabled badge holders

Residents who have a Blue Badge may not need to apply for a residents permit. Blue badge holders are exempt for up to 3 hours on yellow lines where there are no loading restrictions in operation provided their vehicle does not cause safety or congestion problems. Alternatively they can park in any residents', pay and display or shared use bays free. They must ensure that their blue badge is properly displayed.



FREQUENTLY ASKED QUESTIONS

Why is there a charge?

Legislation requires CPZ schemes to be self-financing. This means that it is not possible to enforce the CPZ without making a charge for the resident permit. The charge will need to cover the scheme, administration and enforcement costs. Should the scheme produce a surplus, it goes into a 'parking fund' which is primarily used to fund the concessionary fares which provides free travel for elderly and disabled people.

Do we have to have ticket machines, signposts and lines?

To enable a CPZ to be enforced, it is necessary to install ticket machines, road markings and signs. However, we are very conscious of the street scene and we will always try to minimise the visual intrusion of the equipment. A positive aspect is that controlled parking zones can improve the visual environment by reducing the number of parked vehicles.

We don't have parking problems, so why include my road?

It is probable that streets just outside the zone will have an increase in the number of parked vehicles, as commuters/visitors will still be able to park without time restriction or payment in uncontrolled streets. It may be in your interest to be part of the parking zone rather than just outside it.

What about loading / unloading?

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Will the scheme guarantee me a space outside my house?

No. A CPZ cannot reserve specific parking spaces for individuals. However, the removal of all day parking by people outside of the area should make it easier to park near your home.

SCHEME PROPOSAL

Review of the existing zones

We have reviewed the existing two zones and have made some amendments to the bay types to maximise the parking capacity on street.

Our initial design has been formulated following a review of previous correspondences, a site survey and two meetings with local Residents Associations and Councillors. These designs are not finalised and we will consider all comments received during this consultation.



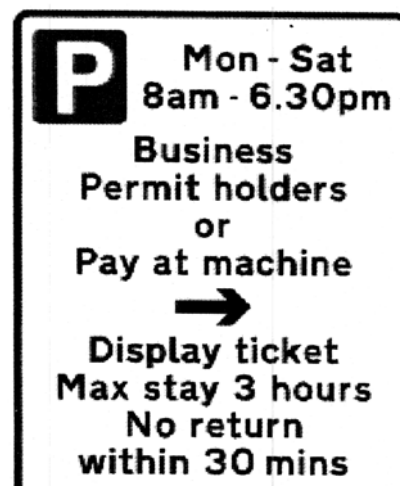
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Please tick the appropriate box.

Q1. Are you a:

Resident

Business

Both

Q2. Since the introduction of parking controls have you found parking in your street to be:

Much easier

Easier

No different

Harder

Much harder

Q3. Do you think the hours of parking controls should:

Remain the same

Be longer

Be shorter

Q4. If you do not want them to remain the same, which of the following would you prefer:

One hour a day (e.g. 3pm to 4pm as per existing zone B)

One hour during am and one hour during pm (e.g. 10am to 11am and 3pm to 4pm as per existing zone H)

One hour during am or pm and one hour during the evening
If evening please specify preferred hour _____

8.30am - 6.30pm

Alternative suggestion, please specify _____

Q5. Which days would you like it to apply:

Monday to Friday (as existing in zone B)

Monday to Saturday (as existing in zone H)

Monday to Sunday

**We would like to know what you think of this consultation document.
Was the information provided:**

About right

Not enough

Too much

Thank you for taking the time to complete this questionnaire. If you have any comments, please use the space provided below. If you do not want your response to be available for public inspection please tick here.

Comments

Thank you for your participation

Possible extension to the existing Stanmore Town Centre Controlled Parking Zone (B Zone)

APPENDIX C



We want to know your views

INTRODUCTION

The purpose of this consultation is to ask your views on whether you would like the existing Stanmore Town Centre Controlled Parking Zone (CPZ) (B Zone) extended to include your road. A number of options are proposed for your consideration. These have been formulated to reflect the feedback already received from the local community.

The Stanmore CPZs (Zones B & H) were introduced in 1994, extended in 1996 and have remained unchanged since. Therefore it has been decided to undertake a review to rationalise the parking in the existing zones, maximize the existing parking spaces and consider extending both of the zones.

The B Zone has a one hour control (3pm-4pm) Monday to Friday and pay and display bays (8am-6.30pm) Monday to Saturday in the shopping area.

The H Zone, around the Stanmore Underground Station, operates Monday to Saturday with a one hour control in the morning (10am-11am) and in the afternoon (3pm-4pm).

We are also currently consulting with the residents within the two existing zones, to identify whether there is a requirement to amend the existing zones, and consulting with residents in areas surrounding the H Zone as to whether they would like to be included in this existing zone.

HOW DO CPZs WORK?

CPZs work by ensuring that vehicles park in designated bays during the hours the CPZ operates. At other times parking is unrestricted except where yellow lines operate for longer periods. Any vehicles that are parked illegally during the controlled times are liable to receive a Penalty Charge Notice (parking ticket).

Service/delivery vehicles can load or unload for up to 20 minutes on the yellow lines where there are no loading restrictions and in residents' bays. However, someone must be in attendance at all times to avoid a parking ticket.

PARKING ROADSHOWS

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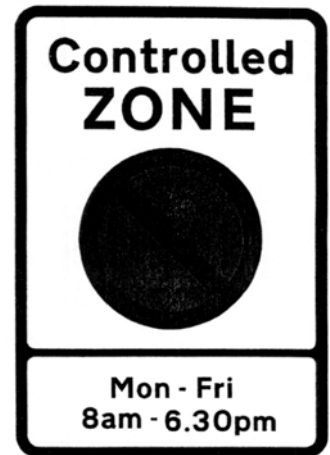
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Project Centre staff will be on hand to answer questions on controlled parking zones and detailed plans of the possible controls will be on display. Please come along.



TYPES OF PARKING BAYS AND WHO CAN USE THEM

Residents permit parking

Bays would be marked for the use of vehicles displaying a valid parking permit during the hours of operation. Residents of the zone would be eligible for parking permits. One permit is needed for each vehicle parked on street during the hours the zone operates.

No. of permits	Cost
1st permit	£40 per year
2nd permit	£50 per year
3rd permit	£70 per year
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Visitor parking

Residents' visitors could park in private driveways where this is possible or arrange their visits outside the restricted time. Alternatively, they may use "Visitor Permits" to park on street during the operational hours of the scheme. It should be noted that accommodation of visitors vehicles within the zone could reduce on street parking provision for residents themselves since they would occupy the same parking bays. Visitors can park in a resident's permit bay during the hours of control provided a valid visitor permit is displayed in their vehicle. Outside the hours of control visitors do not need to display a permit.

Visitor permits come in the form of "scratch cards" and are sold in books of 10. The current cost is detailed below (the operational hours that the proposed zone would operate will depend on the outcome of this consultation):

- 'all day' zone = £15 (£7.50 for senior citizens)
- 'one hour' zone = £10 (£5 for senior citizens)

Each visitors permit would be valid for either the morning or afternoon. Two visitor permits would be needed to park all day.

It should be noted that only residents are eligible for visitors' permits and a maximum of 10 books per household is issued per year (only two can be purchased at any one time).

Business permits

Businesses would be entitled to purchase on-street business parking permits. However, applicants must demonstrate that vehicles are necessary for the conduct of their business.

The number of permits per business will be limited to two. Provision of parking for staff is a matter for the businesses to provide as they see fit. Business permit holders will be allowed to park in specifically signed bays. The maximum stay would be limited to three hours with no return within 30 minutes.

The cost of the permit will be **£300 per permit**.

Shared use bays

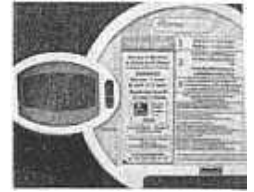
There are a number of different types of shared use bays, which incorporate the three permit groups while providing short stay parking for visitors. The type of bay will depend on the nature of the area. Residents, businesses and visitors must display a valid permit whilst parked in these bays. The maximum stay for a pay and display ticket holder is 3 hours with no return within 30 minutes.



Short stay pay and display bays

Visitors to the area will be able to park in shared use bays. They will need to purchase a pay and display ticket and the length of time they park will be restricted.

The tariff structure for the pay and display spaces will be **40p per half hour** with a maximum stay of 3 hours and no return within 30 minutes.



Parking for disabled badge holders

Residents who have a Blue Badge may not need to apply for a residents permit. Blue Badge holders are exempt for up to 3 hours on yellow lines where there are no loading restrictions in operation provided their vehicle does not cause safety or congestion problems. Alternatively they can park in any residents', pay and display or shared use bays free. They must ensure that their Blue Badge is properly displayed.



FREQUENTLY ASKED QUESTIONS

Why is there a charge?

Legislation requires CPZ schemes to be self-financing. This means that it is not possible to enforce the CPZ without making a charge for the resident permit. The charge will need to cover the scheme, administration and enforcement costs. Should the scheme produce a surplus, it goes into a 'parking fund' which is primarily used to fund the concessionary fares which provides free travel for the elderly and disabled people.

Do we have to have ticket machines, signposts and lines?

To enable a CPZ to be enforced, it is necessary to install ticket machines, road markings and signs. However, we are very conscious of the street scene and we will always try to minimise the visual intrusion of the equipment. A positive aspect is that controlled parking zones can improve the visual environment by reducing the number of parked vehicles.

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SCHEME PROPOSAL

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We have developed parking proposals for your street and wish to identify whether you are in favour of the extension, whether you agree with our design proposals and whether you have any specific preference regarding operational times.

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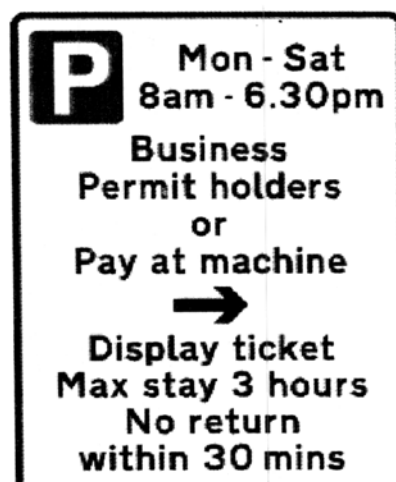
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Yes

No

Don't know

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Q6. Which days would you like it to apply:

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Possible extension to the existing Stanmore Station Controlled Parking Zone (H Zone)

APPENDIX D



We want to know your views

INTRODUCTION

The purpose of this consultation is to ask your views on whether you would like the existing Stanmore Station Controlled Parking Zone (CPZ) (H Zone) extended to include your road. A number of options are proposed for your consideration. These have been formulated to reflect the feedback already received from the local community.

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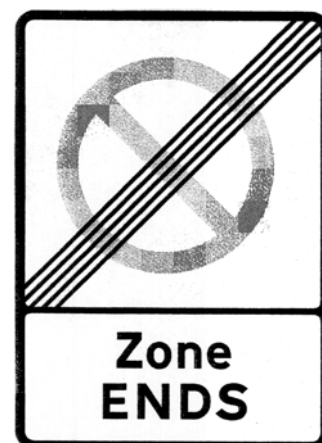
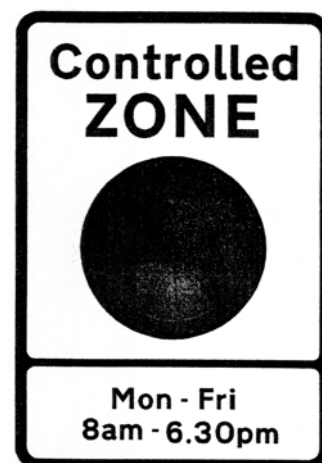
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SCHEME PROPOSAL

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We have developed parking proposals for your street and wish to identify whether you are in favour of the extension, whether you agree with our design proposals and whether you have any specific preference regarding operational times.

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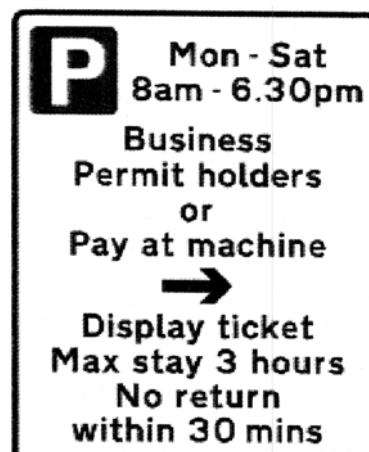
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Q4. If you answered no or don't know to question 3, if parking controls were introduced in the road next to yours, would you then want your road to be included:

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Q5. If the majority of your street voted in favour of being included in a CPZ, which of the following time periods would you prefer:

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Comments

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APPENDIX E

26 August 2004

Urban Living
Executive Director
Tony Lear

Dear Resident/Trader

Proposed Waiting and Loading Restrictions – Stanmore Hill

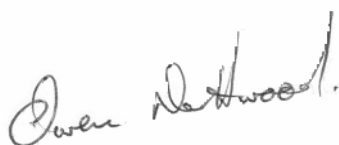
In order to improve the visibility and safety along your section of Stanmore Hill, the Council is proposing to prohibit parking between the hours of 8.00am and 6.30pm Monday to Friday by implementing waiting restrictions (yellow lines).

The purpose of this letter is to consult residents / businesses on the proposal, which is shown in detail on the following page.

Should you wish to comment on the proposals please do so by returning the enclosed questionnaire in the reply envelope by 24 September 2004 (no stamp required). If you require further information or clarification, please contact the project officer, Owen Northwood on **020 8424 1677** or by email at owen.northwood@harrow.gov.uk.

The Council is also undertaking a review of the Stanmore Controlled Parking Zone (CPZ) and this is presently the subject of a similar public consultation. This will not conflict with the waiting proposal mentioned here on which you are being consulted separately from the CPZ review in order to address the safety issue without further delay.

Yours sincerely,



Owen Northwood
Engineer, Traffic Management

Tel: 020 8424 1677

Email: owen.northwood@harrow.gov.uk

Fax: 020 8424 7662

This page consists of an ordnance survey map which is not available electronically.

Stanmore Hill – Proposed waiting and loading restrictions

*Please return in the reply paid envelope provided to reach us by **Friday 24th September 2004.***

I support the proposal as outlined in the letter and as shown on the drawing

I broadly support the proposal but would like to comment as follows: -

.....
.....
.....
.....

I do not support the proposal as shown for the following reason:-

.....
.....
.....
.....
.....

Name Address

Please note that all replies will be considered as public documents unless an individual prefers it to be considered confidential.

If you wish your reply to be considered confidential please tick here

Thank you for your help.

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APPENDIX F

STANMORE STAKEHOLDERS MEETING BERNAYS MEMORIAL HALL

FRIDAY 12TH MARCH 2004
4.00PM – 6.00PM

Present: Alistair Turk (Project Centre)
Gordon Walker (Project Centre)
Paul Ryan (Project Centre)
Graeme Smith (Project Centre)

-

Councillor M Ashton
Councillor D Ashton
Councillor C Bath
Mr Campioni (Capra)
Mr B Kregor (Dean Court Residents Association) x 1 guests
Mrs Lis (Elm Park Residents Association)
Mrs Gordon (Greensward Properties, Kerry Court)
Mr Pearleman (Kendle Residents Association Ltd) x 2 guests
Mrs Noble (Laburnum Court Residents Association, Stanmore, Ltd)
Mr Bharwaney (Orchard Court Residents Association)
Ms E Moss (White House Drive Residents Association)
Mr T Raymond (The Stanmore Society)
Mr H Garfield (Warren Fields Management Ltd)
Mrs Mann (The Berneys Management Group)
Mrs Piazza (Arran Drive)
Mr Harvey (Arran Drive)
Mrs Selata (Arran Drive)
Mr Franks (Arran Drive)
Mrs Curner (Arran Drive)

Alistair Turk opened the meeting with introductions and explained the purpose of the meeting which was a fact-finding exercise to raise and identify parking problems in the Stanmore area. This information would help Project Centre design possible solutions that would be included in a forthcoming public consultation.

Cllr M Ashton gave apologies for other Councillors who could not attend due to late notification of the meeting and who would want to be part of this process. Councillors already have letters of complaints of parking problems.

Explanations were given by **Cllr M Ashton** that there were time constraints as this project was funded by TfL and the funding would have to be spent within the time limits or else taken away.

Alistair Turk informed the meeting that the funding from TfL this year was to carry out the review and consultation and to report the results of the consultation to the Council only. This was the reason the meeting was being held to make sure the Project Centre would get all the correct information.

Concerns from stakeholders

Following Alistair Turk's introduction, comments were asked from the floor

The **Kendle Residents Association** representative stated that parking was disastrous. He also stated that they did not need a meeting as the Council should have records of the correspondence that residents had already sent.

Alistair Turk stated that Project Centre had some copies of letters that the Council had received, but wanted to make sure that we were up-to-date with all of the issues. He also stated that we had looked at the letters and had identified a number of key issues, but that this meeting was to re-affirm and hear for ourselves the exact issues so they could be considered as part of the main consultation process.

A number of attendees stated that Project Centre should already know the problems and issues on parking by past correspondence and confirmed that it is impossible to park in the area.

Residents stated that since the Amora redevelopment on London Road it has made parking matters worse.

A number of residents referred to issues with Dennis Lane. It is very narrow with commuters parking there and that it was being used like a car park. They stated that this made it impossible to see traffic when exiting Laburnum Court.

Cllr M Ashton stated that it had already been agreed that double yellow lines would be installed in Dennis Lane.

A number of issues were raised regarding Arran Drive

One resident stated that the area near Kelmscot required double yellow lines as vehicles park on both sides of the road and obstruct vehicles travelling down the road.

Arran Drive residents enquired if it would be included in the proposals and if so they would like yellow lines 1 hour in the morning and 1 hour in the afternoon. The problem they have is commuters parking. There are no problems at the weekend. Arran Drive also has access problems where refuse trucks and emergency vehicles cannot get through. **Alistair Turk** stated that it was planned to include Arran Drive as part of the study area.

A resident of Kerry Court (opposite the station) stated they have problems with vehicles parking with drivers sitting in their vehicles, waiting to pick people up, with their engines running. This generally happens in the evening (between 4pm-7pm) when residents return home. She stated that residents often cannot find a space to park.

A discussion was had regarding the fact that many residents have more than one vehicle and there is not enough space on street for vehicles to park. A number of people stated that they have garages but that there aren't enough garages for the amount of properties and this is therefore forcing people to park on the street.

One of the main issues that raised throughout the meeting was the LIDL Car Park

Cllr M Ashton stated that nothing should be done before the issue with the LIDL car park was resolved as this would resolve other parking problems. She asked whether Project Centre had been asked to look at the car park as part of this brief.

Alistair Turk stated that although PCL was aware of the car park and that much of it was condemned, it was not part of the brief, but that he was aware of leasing issues with LIDL.

Residents stated that there was a lack of existing parking space in the town centre area and that the issue with the LIDL car park was not helping.

A question was asked as to the possibilities of a multi storey being built? **Alistair Turk** stated that there were a number of options open to the Council, but he could not confirm what would be done as he was not party to this, but that funding is often an issue.

Cllr C Bath stated that as part of the planning conditions Sainsbury's had put aside £385,000 for the development of the LIDL car park and that this money was ring fenced.

Alistair Turk concluded by saying that he wasn't aware of this or any timescales for the development of the car park.

One resident stated that Sainsbury's had even given a list to their staff of roads that they could park in legally.

Residents stated that Council had said that they have no money to patrol the existing restrictions, and that parking enforcement was sporadic.

A question was asked as to whether provisions would be made for visitors (work force) to park when visiting properties? **Alistair Turk** referred to the option of visitor permits or bay suspensions. It was stated that options to residents with regards to the types of available parking like visitor permits are not widely known in the community and should be publicised more.

This led on to a discussion regarding bay suspensions. Project Centre were informed by one of the residents that she had been told that Harrow did not run a suspension scheme. A removal company had tried arranging it and had been told by Harrow that they did not do this. The removals company had stated that they didn't know of any other borough that did not have a suspension policy. **Alistair Turk** also expressed his surprise at this as he also thought most, if not all, London Boroughs had a policy of suspending bays.

A resident complained that they had a problem with vehicles obstructing driveways, and they had contacted the Police who told them that they should contact the Council. When they did so, they were told that the Council could do nothing; it was a Police matter. **Alistair Turk** stated that the Council were correct, as obstruction is still a Police issue. He stated that the Council could only act if a vehicle was parked illegally on a waiting restriction, by issuing a ticket, and if the Council had the provision, they could remove the vehicle to a car pound.

A question was asked as to whether the survey carried out by TMS Consultancy on safety around the Broadway area would be taken into account with any of Project Centres plans? **Alistair Turk** stated that he was not aware of such a report, but would request a copy from Harrow to refer to.

Mr T Raymond (Stanmore Society) gave a brief history of the introduction of CPZs in Stanmore. He stated that they were originally set up before there was a parking problem, and that they didn't want the scheme but were told it was going in and its main aim was to make revenue for the council.

Alistair Turk and **Cllr M Ashton** discussed the issue of displacement. **Cllr Ashton** stated that in her experience new Controlled Parking Zones may help but may also move parking problems to the next area (displacement).

Alistair Turk went on to inform the meeting that there were already issues of vehicle access to streets, times of existing controls, parking around the shopping area, parking around the college area and also problems with commuter parking.

If controls were installed then residents would have to pay for a permit to park during the times of controls. Any design of controls would maximise parking space but allow for the free flow of traffic. He stated that CPZ's were a crude form of parking control, but often effective. It came down to an assessment of the residents need to have prioritised parking against having to pay for this privilege.

Cllr D Ashton asked what were the possibilities of highlighting to TfL to make more provisions for parking at the station car park which would alleviate some parking problems? **Alistair Turk** said that they could try. (Note: This would need to be discussed with Harrow Officers). He also pointed out that the stations are not obliged to make more provision.

Residents complained that vehicles used to park all over the place when there was an event on at Wembley Stadium and travel by Underground to the Stadium. There were concerns that the situation would worsen when the new stadium opens, having a proposed smaller car park.

Alistair Turk confirmed that the car park was indeed smaller and that it was felt that more people would use public transport to get to the ground. He then went on to explain how other event day schemes worked. (Note: - an event day scheme may need to be considered in the future).

It was asked if controls were implemented, would people park in the un-adopted roads. **Alistair Turk** stated that If residents experience parking problems in un-adopted roads, this would have to be addressed by the residents of the road and would not be under the Councils remit. Possibilities to resolve this could be the resident employing private contractors to enforce the area.

Residents asked whether proposed developments (i.e. Spur Road Development) would be taken into account in any design. **Alistair Turk** stated that they generally would, but that they he had not been informed of any developments.

A number of residents stated that they felt that avenues of information had been kept from Project Centre.

Cllr C Bath asked the timescales of this project and stated she felt that if TfL can lay down the rules they could disagree with the outcome of the consultation. **Alistair Turk** said that Harrow at present has not got the funding to implement any new zones. This was supported by **Cllr M Ashton**.

Alistair Turk stated that TfL are only funding the review in this financial year. Any changes or extensions would then be subject to a bid for funds to implement the changes in the next financial year.

Cllr D Ashton asked if it was possible to see Project Centres brief for this job and if there were any further meetings planned and stressed the importance if Project Centre could find a way to highlight the importance of a multi storey car park. (At the end of the meeting **Alistair Turk** showed **Cllr D Ashton** the brief from Harrow).

It was asked whether Project Centre would be publishing the results of the meeting. **Alistair Turk** stated that he would report back to Harrow on Monday and they would decide the next steps. He stated that PCL had offered to have a meeting with the Members to explain the details of the meeting.

Alistair Turk concluded the meeting by stating that the next planned step would be the public consultation in which there would be road shows held for the public to see initial designs. He stated that a leaflet would be sent to all residents within the agreed area. It was stressed that all residents and businesses should complete the consultation document.

**STANMORE FORUM MEETING
BERNAYS MEMORIAL HALL**

**WEDNESDAY 2ND JUNE 2004
4.00PM – 7.00PM**

Present Peter Hazzard (Project Centre)
 Paul Ryan (Project Centre)
 Steve Bond (Project Centre)

 Councillor M Ashton
 Councillor D Ashton
 Councillor A Pinkus (Belmont)
 Councillor P O'Dell (Environment and Transport Portfolio Holder)
 Councillor R Romain (Canons)
 Councillor J Cowan (Canons)
 Mr M Nekousad (Harrow Borough Council – present in an unofficial capacity)
 Mrs Lis (Elm Park Residents Association)
 Mrs Wiener (St Lawrence Close Residents Association)
 Mr and Mrs Phillips

Cllr M Ashton opened the meeting with introductions and explained the purpose of the meeting which was to engage local stakeholders in a discussion on parking problems in the Stanmore area, discuss the consultation materials prepared by The Project Centre and to air any resident concerns. This information would help PCL design possible solutions that would be included in a forthcoming public consultation.

Cllr M Ashton gave apologies for other Councillors who could not attend the meeting and who would want to be part of this process. These included Councillors C Bath, C Bednell, M Kara and J Miles.

Explanations were given by **Cllr M Ashton** that there were time constraints as this project was funded by TfL and the funding would have to be spent within the time limits or would no longer be available.

Mrs Lis commented that the LIDL car park is the No. 1 issue and could be seen as the problem, and the solution, to the areas parking problems. Money from Sainsburys (£385,000) is available to use but expires in 12-18 months time. She thinks the problems are not being looked at directly.

Peter Hazzard accepted that the LIDL carpark was a major issue but pointed out that it is not part of PCL's brief.

Cllr M Ashton reiterated that LIDL car park is not in PCL's brief and explained she recognised there are problems in the area and supports the scheme around the station and college.

Mrs Lis mentioned that cars from the college used to park at the location of the present Sainsburys car park but are no longer able to.

- **Paul Ryan** explained how the existing P&D bays will become Shared Use bays with provision made for resident and business parking. Explained conditions of use and PCL's approach to bay allocation.

Cllr Romain stated he had become aware of a proposal to convert part of the Stanmore Station car park into a storage/shunting area with the loss of spaces for up to 80 cars. Concerned that the overflow from this will affect the areas on the edge of the already saturated zone (e.g Sandymount, Merrion) and increase traffic around the Canons Park station. Realises it is not an issue at this stage but raised concern about timing of the consultation leaflet with regards to this proposal and whether the public will be informed of it.

- **Peter Hazzard** noted that careful wording of such proposals is necessary and stated that PCL would attempt to obtain further information regarding this matter.
- **Cllr M Ashton** commented that since car park closure, business parking has been displaced once already and, having adapted, will do so again.
- **Paul Ryan** stated that disregarding the matter of the car park, parking will be maximised with provision made for business parking. Pointed out that PCL are independent consultants there to represent the views and concerns of the public in an unbiased manner and not there to 'sell anything'.
- **Cllr M Ashton** asked for Cllr O'Dell to explain the latest position regarding the car park.
- **Cllr O'Dell** explained there were numerous leasing issues involved which were taking some time to sort out and that a report was expected soon. He accepted that the car park is a major issue but feels other issues (eg. on-street parking) need to be dealt with also. Noted that there have been complaints from residents in other areas and feels it is an ideal time for a review of the entire area. Expressed interest in the consultation process and hearing public opinion, both positive and negative.
- **Cllr M Ashton** raised concerns over the effect the car park issue will have on the public's response to consultation – they may overreact. If car park issue resolved may end up with more parking restrictions then required. Asked if the consultation process had to include the town centre at this stage and suggested that, if it did, PCL mention the car park issue in the consultation leaflet.

Paul Ryan replied that, again, careful wording would be necessary if this was to occur. Also stated that he was quite disappointed at the low turnout and felt an opportunity had been lost. PCL had been hoping to obtain crucial feedback on the initial design drawings which would assist them in preparing a design that would go some way towards dealing with the issues for local residents.

- **Cllr M Ashton** replied that many people are apathetic following lack of progress with regards to the car park, and that the public struggle to see past this issue. She asked if there were any further questions. As none were forthcoming, she thanked those attending and also thanked PCL for the work they had prepared.

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APPENDIX G

Stanmore CPZ Review – Exhibition Comments

Saturday 11th September 2004 – Stanmore Library

Name and Address	Comment
Newman 16 Talman Grove	This is a small Close with several people owning more than one car. We would absolutely disagree with the scheme, which would generate arguments between neighbours and severely restrict contractor space for parking and almost completely restrict visitors parking. This simply compounds any problems currently in existence
Sandler 22 Gleneagles	This will exacerbate an already crowded parking area where many residents have more than one car and visitors will have to climb to the top floor to obtain a pass and then return to the car during which time they could be fined. This will cause nothing but trouble between neighbours and seems bloody mindless by the council
Clarke Hill Close	The proposals reflect the residents requirements, as they will prevent commuters into London and visitors to the nearby pub preventing residents from parking near their homes
Mendoza 6 Ray Court	"Fingers crossed" the proposals will be approved and implemented without delay
Gasson Gleneagles Gordon Avenue	This is not wanted by most people in the area
Silver 50 Snaresbrook Drive	Some restrictions are desperately needed in Snaresbrook Drive to restrict all day commuters and very thoughtless parkers who frequently block the road so no vehicles can access the whole road. However, as we are at the very end where the footpath is, we are concerned about the proposal as laid out in your plans. This would leave us with nowhere to park near to our house.
Kenton 36 Regents Court Davis 38 Regents Court	The scheme is fairly obviously going to take place, therefore when erecting the signs, PLEASE make Regents Court one-way from the south end to prevent drivers using it as a race track in the morning and evening rush hours, also to prevent head to head confrontations which happen frequently.
G D H Hicks 28 Sunningdale Close	I deplore the whole concept so far as Sunningdale Close is concerned. Moreover, an error has occurred in the marking of the red parking bays – one of them is right across my access and must be rectified if any parking restrictions are applied.

Name and Address	Comment
Noel Gellman 9 Rosedale Close	<p>As explained to me your proposed plans of a single yellow line for many yards at start of Close plus parking bays will reduce the number of cars that can park during the restricted hour whether we have a permit or not. This patently is a total nonsense especially as all the residents of Rosedale Close have no parking problems still allowing safe access for large trade vehicles even the garbage van manages to have access!!</p> <p>Visitors to flats during restricted hour must park, climb up stairs to perhaps top flat – no lift – go back to their car with visitors permit and maybe find that they have been booked!</p>
G Reynolds 15 Burlington Park House Dennis Lane	<p>I do not think any further taxes on the motorist are needed. If the scheme is to proceed there should be a reduced charge for the senior citizens who need permits. I suggest the whole scheme is a way of raising money.</p>
Cole 17 Stanmore Hill	<p>Opposed to the whole scheme. Very ill conceived and not thought out</p>
I Paul 7 Fallowfield	<p>Another of Ken's lunatic schemes. No reason to restrict parking in Fallowfield. All you need to do is restrict parking to one side at the entrance to Fallowfield if parking is allowed on two sides two cannot pass. What a waste of money</p>
Mrs Benson & Mr Doyle 12 Wolverton Road	<p>Oppose scheme in Wolverton / Savernake as there are no current problems at all. CPZ would create problems where there are none</p>
23 Silverston Way	<p>Whilst there are some parking pressure points in the area of Stanmore these proposals appear to be excessively complex and restrictive. The costs of the whole exercise to deal with these proposals seem to be a complete waste of money which ultimately comes out of our (the tax payers) pocket. These answers are not the answer</p>
M & A Howard	<p>There is no problem in our road. Please do not create one. Behind LIDL's is an empty car park. Re-open it and this would clear most of the parking problems, you should have thought about the parking before allowing Stanmore Park to be built. How many cars belong to this complex??</p>
26 Silverston Way	<p>Totally opposed to the entire proposal. To add insult to injury is the proposal to put Double Yellow Lines in front of the properties in the cul-de-sac</p>

Name and Address	Comment
27 Pangbourne Drive	Pangbourne Drive is a road wide enough to accommodate cars either side, however in the CPZ plan yellow lines have been painted along either side of the road with provision for only a few parking zones. As the road has no parking problems at present this seems a bit strange. Upon questioning I was told that this was because of certain planning laws that have to be applied, however they are not set in stone and are subject to residents views. I therefore suggest that the parking bays are extended to all along the road whilst being removed from the proposed area around the "roundabout" in the middle (as these narrow the road and make it dangerous – a fact that cannot be seen on the plan). If these reservations are not taken into account it would be questionable that the council's main priority would be road safety and traffic calming / organisational reasons and more about restricting parking to force people to park on yellow lines and collecting the fines that ensue.
53 Stonegrove Gardens	Need to ensure that ambulances can turn around unhindered. Small estate, many old people (written by Peter Hazzard)
15 Temple Mead	DO NOT WANT THIS SCHEME TO GO AHEAD AT ANY COST!! Harrow Council appear to want to put more and more restrictions on us, in order to obtain more and more money – particularly from pensioners!!
33 Embry Way	This is a cul-de-sac, thus there is no through traffic and there are no street parking problems. Therefore parking restrictions will be a total waste of time and money.
22 Wychwood Close	Please – No. Control volume rather than patch the problem. If infrastructure cannot support traffic – stop building development and build more car parks
14 Sunningdale Close	There is no need for this parking proposal as there is absolutely no problem in our close or Gordon Avenue. We are at least half an hour from Stanmore Centre. There are no schools, shops, transport or places of worship nearby.
Demetriou 6 Lansdowne Road	We have no problems in this road, restrictions are unnecessary. The money would be better spent on reducing the speed at which motorists drive in this "short cut" route before a child gets killed.
31 Beatty Road	We are not for the scheme. We have no problem with parking at present – but could foresee it if the plan goes ahead

Name and Address	Comment
5 Buckingham Parade	No need for all this at all. Give us enough car parks. Don't shut them down or sell them to supermarkets. What's happened to the nearly million pounds given towards LIDL's car park? Where do the people who work in Stanmore park? Spend our money wisely. Not on meetings & ridiculous suggestions
J Prett 41 Belmont Lane	No problem with Parking – no need for expense of lines, machine etc for road which will detract visually from the area. What happened to the proposed car park in Stanmore?
37 Peters Close	No need for anything – let alone double yellow lines! We have had emergency vehicles, dustcarts etc - NO problem
11a Silverston Way	We live in a quiet Road – never any problem parking – dustcarts get through ok – NOT a necessary scheme! Please sort out parking in Whitchurch Road
14 Golf Close	We are at risk of flooding and do not want any more concrete front gardens. Golf Close does not have a parking problem and is half an hour from the station.
13 Golf Close	Two old age pensioners – front garden sealed with wall to stop further flooding (last flooding Sept 93) – no parking space allocated to our house – what in heavens name do we do with our cars every day!
87 Wychwood Avenue	No CPZ !! We live in a quiet road, no parking problems. CPZ would create problems
Wood Lane	I object in principle to the extension of the CPZ. We have no problem with people parking. I see this extension as a money making exercise and nothing else
Marsh Lane	I have no problems with parking, who decided this operation? I think this is a form of finance for the council
Embry Way	I object we have no problem with all day Parking and it will cause more inconvenience as a lot of residents are elderly and have visitors and family coming and going at all times
127 Stonegrove	There is no problem with all day parking. We are too far from the station but there is a problem especially at peak times when school coaches come through, with traffic flow in the slip road at the end of Pangbourne Drive and Stonegrove. The slip road needs to be one-way with a double yellow line on one side of the road. There is no problem with unlimited parking on one side of the road. (refer to map in comments book)
125 Stonegrove	I object and have no problems with all day parking on the road also I do agree with the proposal from 127 Stonegrove fully.

Name and Address	Comment
Mr & MsD'Souza Golf Close	I am registering a complaint about the roadshow dates and times. As an Orthodox Jewish family we cannot attend on Saturday or the Jewish New Year. Bernays Hall is used for Jewish Holidays so how can you have a roadshow on the same day? The dates and times are totally biased against the Jewish Community attending.
S Calderbant 8 Golf Close	I wish to say that this consultation and survey are flawed with errors. The survey questionnaire should have had a question asking if you wanted to say no. Also the Roadshows are both on days which prevent the Jewish Community from going to express their views. I object to the proposal of a CPZ in my road. It would ruin the area
??	We totally object to this ridiculous money making scheme. There are no problems in Winscombe Way and this proposal has upset not only residents of this road, but St John's school parents only

Stanmore CPZ Review – Exhibition Comments

Tuesday 14th September 2004 – Bernays Memorial Hall

Name and Address	Comment
Mrs Patricia Goldstone 2 Old Forge Close	The “No parking” hour should not be “3-4pm” near to schools such as St Johns Stanmore Hill as it is when parents collect their children fro school. Any other hour in the day is preferable. 3-4 is particularly difficult for residents
Mr Leslie Lightman 76 Howberry Road	Why no parking 3-4 pm in Howberry Road after 2 nd Roundabout if first two sections are no parking 2-3pm. This will only confuse people.
R Miller 15 Lansdowne Road	The council must take action to prevent parking outside the schools in Abercorn Road and Wemborough Road before a serious accident occurs. No CPZ is required in Lansdowne Road
Peter Goldstone 2 Old Forge Close	There is no parking problem. Extending it to our area would affect parents collecting children fro St Johns School – unless there is no parking on Stanmore Hill above the present limit
S Kaye Lemark Close	All the residents of Lemark Close will send in their questionnaires by 21/9/04. Very Severe parking problems
N Gray 4 Peters Close	Nobody wants double yellow lines here. Parking is not a problem. The best way to improve access for emergency vehicles is to widen the road. The pavement o/s no. 4 is often mounted by large vehicles and paving stones cracked. There is no point in relaying the same type. There are frequent examples of speeding in Howberry Road end. Could some speed ramps be installed before someone is killed.
C Chilton Pangbourne Drive	We have absolutely NO PROBLEM with parking. The Road is very wide with good access points along it. The school pick up / drop off is only for half an hour at most. A significant number walk to school for the access to Pangbourne Drive end so why penalise the residents!!! We should not have to pay for parking outside our house and our friends. Is this just a money making project for Harrow Council?? No one leaves their car down Pangbourne all day long. We do NOT get any cars from the station parking at all. Why create the problem of parking then down our road. A better use of your time would be to reduce the speed of traffic along Pangbourne

Name and Address	Comment
E Levy 9 Morecambe Gardens	The new H boundaries to include Brockley Hill, Stonegrove etc are far too wide. For the proposed charge of £40 per annum, it will be a very cost saving way of getting to Stanmore station and slogging up surrounding streets. Also when Wembley is officially finished, Morecambe Gardens will need Sunday and evening restrictions in addition to those already in place.
E Sherbourne 9 Wildcroft Gardens	Not needed in this road
M Jones 15 Colman Court	Not wanted at any price. Parking an extra tax on residents
George Summerfield Oak Lodge Close	You need to address the real problem by opening the multi-story above LIDL. Will write to Harrow Council
Gold 43 Pangbourne Drive	We are constantly plagued by parking from the commuters who park from early morning to late evening for Stanmore Station. Then we have school staff, and pupils who attend Aylward Beauty College who park in the road and very often over our drive way and when you confront them they often use abusive language. The quicker they introduce this scheme the better.
9 Glebe Road	Because Glebe Road (at Broadway end) is so narrow a stronger restriction on the yellow line is needed. If any vehicle parks opposite parking bays the road is completely blocked. Commuters do use the parking bays and regularly park from 8 / 8.30 am to 2.55pm. Another hour in the am period would solve this problem (hopefully)
Roy & Edith McCathie 9 Gressenham Crescent Arran Drive	Opposite Brompton concerned about the length of the proposed yellow line – would like to see the parking spaces maximised this. Also, would like the length of the disabled bays to be made standard sizing (one is much longer than the others)
Nigel & Carol Curtis Green Corner Green Lane	Any restrictions must recognise the problem for parents collecting and delivery children to St John's School. Consideration should also be given to the widening of Green Lane where the grass verges are for the parking bays to avoid the slalom course which currently exists when coming down Green Lane.
Simone & Neal Lester 23 Dovercourt Gdns	More research is needed – consider Edgware & District Reform Synagogue in Stonegrove Barnet. There are up to 100 cars parked on the Harrow side of the road at various occasions during the week.

Name and Address	Comment
	Where are passengers suggest to park if using the tube as encouraged by Government. Also consider what will happen when Wembley Stadium comes in to use.
Mrs K Bezin 11 Greyfell Cl	Greyfell Close is a small Cul-de-sac. Each tenant has their own allocated space and is very happy with the status quo. Our close is tucked away and we have no parking problems.
K Brown 89 Belmont Lane	Belmont does not need to be included as this is not a problem. If part of the road is to be included then anyone who needs to park will 1 st move further down the road to be outside the CPZ
Mr & Mrs T Reid 10 Fallowfield	We feel that parking (day & night) on both sides of the road at the top of the road is a safety hazard. The cars parked on Stanmore Hill on the left and right of Fallowfield block any view of the road in both directions. In Fallowfield at the top end parking should be on one side only at all times to enable access for any emergency vehicles. The remains of the road should have no parking for one hour am and one hour pm. We feel there is no need for resident parking bays.
Dorothy Robins 5 Savernake Court Wolverton Road	I am not aware of a parking problem at present. Should there be double yellow lines at the bottom of the road there would be an overspill. I also would object strongly to paying outside my home. There must be other methods.
Cowan 3 Brockley Close	There is no problem in this road. I would strongly object to having double yellow lines outside my house or having to pay to park
Gersttler 1 Laurimel Close	We do NOT want parking restrictions in Laurimel Close, but are very eager for them in September Way – it's a nightmare!!!
Beach 17 Peters Close	We DO NOT want a controlled zone, it is not necessary in Peters Close. We have no problems with emergency vehicles. Workman and visitors will be forced to use Howberry Road residents bays.
Kraft 12 September Way	There is a problem with Students from Stanmore College. Emergency Vehicles cannot get through also abuse and mess from students.
M Flower Red Tiles Green Lanes	Residents only scheme (least worst option) making the road one way only from the Uxbridge Road should be considered. As a conservation area I would not like a controlled zone at all.

Stanmore CPZ Review – Exhibition Comments

Thursday 16th September 2004 – Bernays Memorial Hall

Name and Address	Comment
Steele 3 Green Lane Cottages	Residents only scheme best option. Current suggestion will cause much upset. Leave road alone as we have managed for 40 years like this! One way up or down or sleeping policemen or 6' barriers have all been marked down before. "ban school run" 3-4pm. STOP STEALTH TAX!!!!
Weston Green Lane	The parking on both sides of the road outside the cottages near the top is a particular hazard because of the narrowing of the road at that point – particularly bad at school collecting times. It can sometimes be dangerous and results in gridlock. I think there should be restricted parking of some sort instated
J Sadler Sandymount Avenue	Parking opposite an already parked car giving less room for other vehicles that are larger than areas left. We have had damage to our car due to this in the last 6 months. We would like to have Wembley events taken into consideration for the future Wembley Stadium. Remember less room can be dangerous as emergency vehicles have trouble getting up and down our road as well as council vehicles
D Shah 7 Calthorpe Gardens	Satisfied with all the explanations. I would agree for go ahead on the project. All questions that I had duly clarified. I am happy to go along.
Mr Nash Peters Close	Don't want Yellow lines. Have no problem parking. Concerned that residents will have nowhere to park. Also concerned that nature reserve will not be used due to no parking facilities. Also felt that the leaflet was misleading. No mention of the possibility of Double Yellow Lines in narrow roads
23 Peters Close	No need has been shown for any extension to the CPZ as a whole, but this question has not been asked. Double Yellow lines in Peters Close – NO!
P Chundasama 16 Goodhall Close	I do not want parking restrictions on my estate, however has any thought been given to the small park in Stanmore Park and its potential on Parking in the area
S North 53 Cheyneys Avenue	NO CPZ NECESSARY IN CHEYNEYS AVENUE – if restrictions at all – would prefer extension of the current system at lower end of Cheyney Avenue to upper end i.e. yellow line only with 1 hour restriction NO BAYS or METERS. I would object strongly to a bay o/s my house (no. 53) as I currently maintain the verge to a high standard and sweep the street outside my property

Name and Address	Comment
M C Cohen 6 Aberdeen Cottage Belmont Lane	There is absolutely NO parking problem on my road, I am over a 20 minute walk from any shops and there is no need for a CPZ. This is just another appalling way to tax us and is ridiculous. I am STRONGLY opposed to this going ahead and if there does have to be any action I will be extremely surprised and disappointed!! We pay enough tax already and Harrow Council do not provide a good enough service in other areas to justify this! NO CPZ!!
Cohen 84 Old Church Lane	We do not want the CPZ to go ahead!! We already pay enough tax and have no parking problems in our road. NO CPZ should be implemented
Cook / Wood 2 Green Lane Cottages Green Lane	The part of Green Lane we live in is not affected by people parking their cars in order to work / shop in Stanmore Town Centre. There is definitely no reason to have a CPZ scheme in our area of Green Lane. There are other issues that we will put forward on our brochure reply
Korn / Morris 9 Courtens Mews	<ol style="list-style-type: none"> 1. There is no parking problem in Belmont Lane / Courten Mews. 2. This is a money making scheme because the Council has overspent 3. The consultation period is too short 4. One out of the 2 days required to come in is a Jewish holiday and therefore most people wont be able to register 5. Open up old unused car parks and then no parking problems will exist

APPENDIX H

Review of existing zones B and H

ROAD NAMES	Q3: Do you think the hours of parking control should:			Q4: If you do not want them to remain the same, which of the following would you prefer:						Q5: Which days would you like it to apply:							
	No. of responses	No. of properties	% reply	No reply	Remain the same	Be longer	Be shorter	No reply	1- One hour a day	2- One hour during am and one hour during pm (eg. 10am to 11am and 3pm to 4pm as per existing zone H)	3- One hour during am or pm and one hour during the evening.	4- 8.30am-6.30pm	5- Alternative suggestion, please specify	No reply	Monday to Friday (as existing in zone B)	Monday to Saturday (as existing in zone H)	Monday to Sunday
ALBEMARLE PARK	3	25	12%	-	1 33.30%	1 33.30%	1 33.30%	-	1 50.00%	1 50.00%	-	-	-	-	3 100.00%	-	-
BEECH TREE CLOSE	1	15	7%	-	1 100.00%	-	-	-	-	-	-	-	-	1 100.00%	-	-	-
BELGRAVE GARDENS	1	9	11%	-	1 100.00%	-	-	-	-	-	-	-	-	-	1 100.00%	-	-
BERNAYS CLOSE	4	54	7%	-	1 25.00%	3 75.00%	-	-	-	1 33.30%	1 33.30%	-	-	-	3 75.00%	-	1 25.00%
BUCKINGHAM PARADE	4	42	10%	-	1 25.00%	1 25.00%	2 50.00%	1 33.30%	-	-	-	1 33.30%	1 33.30%	1 25.00%	2 50.00%	1 25.00%	-
CHURCH ROAD	5	54	9%	1 20.00%	-	1 20.00%	3 60.00%	2 50.00%	-	-	-	1 25.00%	1 25.00%	2 40.00%	2 40.00%	-	1 20.00%
CLAIRE GARDENS	3	31	10%	-	-	2 66.70%	1 33.30%	-	-	2 66.70%	-	1 33.30%	1 33.30%	-	1 33.30%	1 33.30%	1 33.30%
COPLEY ROAD	5	44	11%	-	4 80.00%	-	1 20.00%	-	-	-	-	1 100.00%	1 100.00%	1 20.00%	3 60.00%	-	1 20.00%
FRIGWELL DRIVE	7	53	13%	1 14.30%	3 42.90%	1 14.30%	2 28.60%	1 33.30%	1 33.30%	1 33.30%	1 33.30%	-	-	4 57.10%	3 42.90%	-	-
GENE GARDENS	17	84	20%	2 11.80%	11 64.70%	-	4 23.50%	1 5.90%	2 50.00%	-	-	2 50.00%	2 50.00%	6 35.30%	10 58.80%	-	1 5.90%
DENNIS GARDENS	6	18	33%	-	1 16.70%	5 83.30%	-	1 20.00%	-	4 80.00%	-	1 20.00%	-	1 16.70%	3 50.00%	1 16.70%	1 16.70%
DENNIS LANE	2	22	9%	-	-	2 100.00%	-	-	-	2 100.00%	-	-	-	-	-	2 100.00%	-
ELM PARK	20	80	25%	-	13 65.00%	5 25.00%	2 10.00%	-	3 42.90%	3 42.90%	-	1 14.30%	1 14.30%	4 20.00%	12 60.00%	3 15.00%	1 5.00%
GLEBE ROAD	13	42	31%	-	8 61.50%	4 30.80%	1 7.70%	-	1 20.00%	1 20.00%	1 20.00%	1 20.00%	3 23.10%	7 53.80%	2 15.40%	2 15.40%	1 7.70%
HAIG ROAD	7	33	21%	1 14.30%	4 57.10%	2 28.60%	-	-	-	1 50.00%	1 50.00%	-	-	1 14.30%	4 57.10%	1 14.30%	1 14.30%
HARDWICK CLOSE	7	48	15%	1 14.30%	5 71.40%	1 14.30%	-	-	-	1 100.00%	-	-	-	1 14.30%	6 85.70%	-	-
INGRAM CLOSE	2	19	11%	-	2 100.00%	-	-	-	1 25.00%	-	-	1 25.00%	1 25.00%	1 50.00%	1 50.00%	-	-
LONDON ROAD	20	103	19%	2 10.00%	14 70.00%	2 10.00%	2 10.00%	-	-	-	-	-	-	4 20.00%	14 70.00%	1 5.00%	1 5.00%
MARSH LANE	17	73	23%	-	15 88.20%	-	2 11.80%	2 100.00%	-	-	-	-	-	2 11.80%	15 88.20%	-	-
MERRION AVENUE	30	145	21%	1 3.30%	25 83.30%	4 13.30%	-	-	-	4 100.00%	-	-	-	6 20.00%	17 56.70%	3 10.00%	4 13.30%
NELSON ROAD	2	11	18%	-	1 50.00%	1 50.00%	-	-	1 100.00%	1 100.00%	-	-	-	1 50.00%	1 50.00%	-	-
OLD CHURCH LANE	7	47	15%	-	4 57.10%	1 14.30%	2 28.60%	-	-	1 33.30%	-	-	2 66.70%	3 42.90%	3 42.90%	-	1 14.30%
RAY GARDENS	1	9	11%	-	1 100.00%	-	-	-	-	-	-	-	-	-	1 100.00%	-	-
RECTORY CLOSE	4	18	22%	1 25.00%	2 50.00%	1 25.00%	-	-	-	-	-	1 100.00%	-	2 50.00%	1 25.00%	-	1 25.00%

ROAD NAMES	No. of responses		No. of properties		% reply		Q3: Do you think the hours of parking control should:				Q4: If you do not want them to remain the same, which of the following would you prefer:				Q5: Which days would you like it to apply:																
	No. of responses	No. of properties	% reply	No. of responses	No. of properties	% reply	No reply	Remain the same	Be longer	Be shorter	No reply	1- One hour a day	2- One hour during am and one hour during pm (eg. 10am to 11am and 3pm to 4pm as per existing zone H)	3- One hour during am or pm and one hour during the evening.	4- 8.30am-6.30pm	5- Alternative suggestion, please specify	No reply	Monday to Friday (as existing in zone B)	Monday to Saturday (as existing in zone H)	Monday to Sunday											
SANDYMOUNT AVENUE	11	63	17%	2	18.20%	2	18.20%	6	54.50%	2	18.20%	2	66.70%	-	-	1	33.30%	3	27.30%	8	72.70%	-	-								
STANMORE HILL	11	80	14%	2	18.20%	2	18.20%	3	27.30%	2	16.70%	1	16.70%	-	1	16.70%	3	27.30%	7	63.60%	1	9.10%	-	-							
THE BROADWAY	16	31	52%	2	12.50%	2	31.30%	5	31.30%	2	11.10%	2	22.20%	1	11.10%	2	22.20%	9	56.30%	3	18.80%	4	25.00%								
THE RIDGEWAY	2	6	33%	-	100.00%	2	100.00%	2	100.00%	-	-	-	-	-	-	-	-	-	-	2	100.00%	-	-	-							
WHITE HOUSE DRIVE	5	33	15%	-	60.00%	1	20.00%	3	60.00%	1	50.00%	-	-	-	1	50.00%	-	-	-	5	100.00%	-	-	-							
KERRY AVENUE	3	7	43%	-	33.30%	1	66.70%	1	33.30%	2	100.00%	-	-	-	2	100.00%	-	-	-	-	-	2	66.70%	1	33.30%						
KERRY COURT	1	35	3%	-	100.00%	1	100.00%	-	-	-	-	1	100.00%	1	100.00%	1	100.00%	-	-	-	-	-	1	100.00%	-						
MORECAMBE GARDENS	4	46	9%	2	50.00%	1	25.00%	1	25.00%	1	25.00%	-	-	-	-	1	100.00%	4	100.00%	-	-	-	-	-							
THE HAGEL DRIVE	2	12	17%	-	100.00%	2	100.00%	-	-	-	-	-	-	-	-	-	-	2	100.00%	-	-	-	-	-							
WASTBERE DRIVE	2	17	12%	-	100.00%	2	100.00%	-	-	-	-	-	-	-	-	-	-	-	-	-	-	2	100.00%	-	-						
PINNACLES CLOSE	2	11	18%	1	50.00%	1	50.00%	-	-	1	100.00%	-	-	-	-	1	100.00%	-	-	-	-	-	-	2	100.00%						
GOODWOOD CLOSE	1	9	11%	-	100.00%	1	100.00%	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-							
NARESBY FOLD	1	8	13%	-	100.00%	1	100.00%	-	-	1	100.00%	-	-	-	-	-	-	1	100.00%	-	-	-	-	-							
TOTAL	249	1437	17%	19	7.60%	144	57.80%	49	19.70%	37	14.90%	13	15.10%	12	14.00%	31	36.00%	12	14.00%	15	17.40%	18	20.90%	66	26.50%	139	55.80%	25	10.00%	19	7.60%

Note: The total number of properties only relates to the roads that have responded

APPENDIX I

ROAD NAMES	No. of responses	No. of properties	% reply	Q3: Are you in favour of parking controls being introduced in your road:				Q4: If you answered no or don't know to question 3, if parking controls were introduced in the road next to yours, would you then want your road be included:				Q5: If the majority of your street voted in favour of being included in a CPZ, which of the following time periods would you prefer:					Q6: Which days would you like it to apply:				
				No reply	Yes	No	Don't know	No reply	Yes	No	Don't know	No reply	1- One hour a day (eg 3pm to 4pm as per zone B)	2- One hour during am and one hour during pm (eg. 10am to 11am and 3pm to 4pm as per existing zone H)	3- One hour during am or one hour during the evening.	4- 3.30am to 6.30pm	5- Alternative suggestion, please specify	No reply	Monday to Friday (as existing in zone B)	Monday to Saturday (as existing in zone H)	Monday to Sunday
ABERCORN ROAD	13	18	72%	-	-	13	-	-	12	-	-	-	-	-	-	12	1	-	-		
ADELAIDE CLOSE	6	37	16%	-	-	6	-	92.30%	7.70%	-	-	-	-	-	-	92.30%	7.70%	-	-		
ARAN DRIVE	26	82	32%	1	21	3	1	-	16.70%	83.30%	-	-	1	1	3	2	2	1	1		
AYLMER DRIVE	11	26	42%	3.80%	80.80%	11.50%	3.80%	-	7.70%	25.00%	1	3.80%	-	7.70%	3	19	3	11.50%	3.80%		
AYLWARDS RISE	1	11	9%	-	-	1	-	18.20%	81.80%	10.00%	-	-	-	-	7	4	-	-	-		
BARN CRESCENT	13	19	68%	-	3	10	-	-	100.00%	-	-	-	-	-	1	-	-	-	-		
BEATTY ROAD	21	145	14%	-	23.10%	76.90%	-	-	10.00%	90.00%	-	-	1	-	5	6	-	-	2		
BELMONT LANE	33	103	32%	-	3	17	1	-	1	16	1	7.70%	-	1	11	8	-	-	2		
BEN HALE CLOSE	4	11	36%	-	14.30%	81.00%	4.80%	-	5.60%	88.90%	5.60%	4.80%	-	4.80%	2	11	-	-	9.50%		
BENTLEY WAY	2	44	5%	-	2	30	1	2	2	27	-	3.00%	-	2	27	6	-	-	-		
BOWLS CLOSE	3	12	25%	-	1	2	1	2	6.50%	87.10%	-	3.00%	-	6.10%	2	81.80%	18.20%	-	-		
BOYLE AVENUE	11	41	27%	-	25.00%	50.00%	25.00%	-	66.70%	-	33.30%	-	-	-	-	-	25.00%	-	-		
CAPUCHIN CLOSE	1	11	9%	-	-	2	-	-	100.00%	-	-	-	-	-	-	-	-	-	-		
CARR CLOSE	2	21	10%	-	1	2	-	-	100.00%	-	-	-	1	-	-	-	100.00%	-	-		
CHAMBERS WALK	3	35	9%	-	2	1	-	-	1	100.00%	-	-	1	-	1	-	-	-	1		
CHANDOS COURT	5	19	26%	-	3	2	-	-	66.70%	-	-	-	3	-	33.30%	-	-	-	2		
CHEYNEYS AVENUE	25	102	25%	-	5	20	-	-	20.00%	80.00%	-	20.00%	-	-	-	-	3	2	-		
CONWAY CLOSE	10	14	71%	-	1	9	-	-	100.00%	-	-	4.00%	-	4.00%	1	4	18	2	1		
COURTENS MEWS	16	28	57%	-	1	15	-	-	12.00%	88.00%	-	10.00%	-	-	16.00%	72.00%	8.00%	-	4.00%		
CRANMER CLOSE	4	7	57%	-	6.30%	93.80%	-	-	93.30%	6.70%	-	6.30%	-	-	81.30%	12.50%	6.30%	-	-		
CULVERLANDS CLOSE	8	31	26%	-	5	3	-	-	25.00%	25.00%	-	25.00%	-	50.00%	25.00%	2	3	3	-		
DEARNE CLOSE	8	22	36%	-	62.50%	37.50%	-	-	100.00%	-	-	12.50%	-	-	25.00%	37.50%	37.50%	-	-		
DENNIS LANE	23	53	43%	1	15	7	-	-	71.40%	28.60%	2	-	1	-	1	6	1	-	-		
DOUGLAS CLOSE	10	34	29%	4.30%	65.20%	30.40%	-	-	14.30%	85.70%	-	26.10%	-	8.70%	2	11	4	6	26.10%		
DOWDING PLACE	2	7	29%	-	-	9	1	-	20.00%	20.00%	2	-	-	-	4	6	-	-	-		
DU CROS DRIVE	18	51	35%	-	2	16	-	-	50.00%	50.00%	-	-	-	50.00%	1	1	-	-	-		
				-	11.10%	88.90%	-	-	6.30%	87.50%	6.30%	16.70%	-	5.60%	6	7	2	3	16.70%		

ROAD NAMES	No. of responses	No. of properties	% reply	Q3: Are you in favour of parking controls being introduced in your road:				Q4: If you answered no or don't know to question 3, if parking controls were introduced in the road next to yours, would you then want your road be included:				Q5: If the majority of your street voted in favour of being included in a CPZ, which of the following time periods would you prefer:					Q6: Which days would you like it to apply:			
				No reply	Yes	No	Don't know	No reply	Yes	No	Don't know	No reply	1- One hour a day (eg 3pm to 4pm as per zone B)	2- One hour during am and one hour during pm (eg. 10am to 11am and 3pm to 4pm as per existing zone H)	3- One hour during am or pm and one hour during the evening.	4- 3.30am to 6.30pm	5- Alternative suggestion, please specify	No reply	Monday to Friday (as existing in zone B)	Monday to Saturday (as existing in zone H)	Monday to Sunday
EATON CLOSE	12	56	21%	1	6	5	-	1	3	3	3	2	-	-	3	7	-	2	16.70%	
ELIZABETH GARDENS	10	70	14%	8.30%	50.00%	41.70%	-	20.00%	60.00%	4	3	2	1	2	6	3	1	-	-	
ELLIOT ROAD	4	30	13%	-	30.00%	50.00%	20.00%	-	57.10%	2	42.90%	10.00%	2	20.00%	1	3	10.00%	-	-	
EMBRY CLOSE	4	7	57%	-	50.00%	50.00%	-	-	100.00%	2	-	-	2	-	1	2	-	1	25.00%	
EMBRY DRIVE	6	34	18%	-	25.00%	75.00%	-	33.30%	33.30%	1	1	-	-	-	1	3	-	-	25.00%	
EMBRY WAY	24	67	36%	-	-	100.00%	-	-	6	6	-	-	-	-	1	5	-	-	-	
FALLOWFIELD	21	43	49%	-	-	95.80%	4.20%	4.20%	83.30%	20	1	1	3	12.50%	12	9	1	2	8.30%	
FALLOWFIELD COURT	2	7	29%	-	14.30%	81.00%	4.80%	-	77.80%	14	1	1	4	-	8	11	1	1	4.80%	
GLANVILLE MEWS	6	16	38%	-	-	100.00%	-	-	100.00%	2	1	-	-	-	-	2	-	-	-	
GLENEAGLES	14	35	40%	1	-	100.00%	-	-	100.00%	6	3	-	-	-	2	4	-	-	-	
GOLF CLOSE	19	26	73%	7.10%	-	92.90%	-	-	100.00%	6	1	-	-	7.10%	9	5	-	-	-	
GOODHALL CLOSE	5	29	17%	-	-	100.00%	-	1	5.30%	17	1	-	-	-	18	1	-	-	-	
GORDON AVENUE	35	71	49%	-	60.00%	40.00%	-	-	50.00%	1	1	-	-	-	3	3	1	2	40.00%	
GREEN LANE	45	123	37%	1	20	21	3	1	3.30%	29	5	2	9	-	20	13	1	1	2.90%	
GREENACRES DRIVE	15	18	83%	2.22%	44.44%	46.67%	6.67%	4.20%	79.20%	8	13	-	20.00%	15.56%	10	29	4	2	4.30%	
GREYFELL CLOSE	4	11	36%	-	6.70%	93.30%	-	-	7.10%	12	1	1	1	-	12	2	-	-	6.70%	
HALL FARM CLOSE	2	12	17%	-	1	1	-	-	100.00%	4	1	-	-	-	3	1	-	-	-	
HALSBURY CLOSE	2	5	40%	-	50.00%	50.00%	-	-	100.00%	1	1	-	-	-	2	2	-	-	-	
HERIOTS CLOSE	2	11	18%	-	-	100.00%	-	1	50.00%	1	1	-	-	-	1	1	-	-	50.00%	
HEWETT CLOSE	5	14	36%	-	4	-	1	-	80.00%	2	-	-	3	1	1	3	-	-	-	
HILL CLOSE	4	29	14%	-	3	-	1	-	100.00%	1	2	1	1	-	2	2	1	1	20.00%	
HODGKINS MEWS	7	7	100%	-	75.00%	25.00%	-	-	100.00%	3	2	25.00%	25.00%	-	-	5	1	1	25.00%	
HOLLAND CLOSE	4	12	33%	-	42.90%	57.10%	-	-	75.00%	4	2	-	14.30%	14.30%	-	4	-	-	14.30%	
HOLLAND WALK	12	28	43%	-	-	100.00%	-	-	100.00%	1	1	-	-	25.00%	1	9	1	-	100.00%	
HOWBERRY CLOSE	11	36	31%	-	-	91.70%	8.30%	-	83.30%	10	2	-	16.70%	-	1	8	2	-	8.30%	
HOWBERRY ROAD	22	75	29%	-	81.80%	18.20%	-	-	100.00%	2	2	-	-	27.30%	4	16	1	1	9.10%	
				-	36.40%	63.60%	-	-	85.70%	12	4	-	-	9.10%	18.20%	72.70%	4.50%	-	4.50%	

ROAD NAMES	No. of responses	No. of properties	% reply	Q3: Are you in favour of parking controls being introduced in your road:				Q4: If you answered no or don't know to question 3, if parking controls were introduced in the road next to yours, would you then want your road be included:				Q5: If the majority of your street voted in favour of being included in a CPZ, which of the following time periods would you prefer:					Q6: Which days would you like it to apply:								
				No reply	Yes	No	Don't know	No reply	Yes	No	Don't know	No reply	1- One hour a day (eg 3pm to 4pm as per zone B)	2- One hour during am and one hour during pm (eg. 10am and 3pm to 4pm as per existing zone H)	3- One hour during am or pm and one hour during the evening.	4- 3.30pm to 6.30pm	5- Alternative suggestion, please specify	No reply	Monday to Friday (as existing in zone B)	Monday to Saturday (as existing in zone H)	Monday to Sunday				
LABURNUM COURT	10	31	32%	-	4	6	-	-	16.70%	83.30%	5	-	1	2	6	60.00%	6	2	7	2	20.00%	70.00%	10.00%	-	-
LADY AYLESFORD AV.	33	47	70%	-	10	21	2	1	16.70%	5	17	1	3	20	4	60.00%	1	3	21	3	9.10%	63.60%	18.20%	9.10%	3
LANDSDOWNE ROAD	44	75	59%	-	6	38	-	1	18.40%	13.20%	25	1	16	15	7	34.10%	7	20	19	45.50%	43.20%	6.80%	4.50%	2	
LAURIMEL CLOSE	4	47	9%	-	2	2	-	-	-	100.00%	2	-	1	2	1	50.00%	1	1	3	25.00%	75.00%	-	-	-	-
LEAVESDEN ROAD	8	26	31%	-	1	7	-	2	-	57.10%	4	2	4	3	-	37.50%	1	3	4	37.50%	50.00%	-	-	1	12.50%
LEMARK CLOSE	9	14	64%	-	9	-	-	-	-	100.00%	-	-	-	1	7	11.10%	1	-	2	22.20%	77.80%	-	-	-	-
LINDEN CLOSE	1	8	13%	-	1	-	-	-	-	100.00%	-	-	-	-	1	100.00%	-	-	-	-	100.00%	-	-	-	-
LINKS VIEW CLOSE	3	10	30%	-	-	3	-	-	-	66.70%	2	-	-	3	-	100.00%	-	-	3	-	100.00%	-	-	-	-
MALCOLM COURT	3	29	10%	-	1	2	-	-	-	100.00%	2	-	2	1	-	33.30%	-	2	1	2	66.70%	33.30%	-	-	-
MARSH LANE	31	126	25%	-	6	23	2	2	-	19.00%	19	2	5	21	5	67.70%	1	22	17	22.60%	54.80%	3.20%	19.40%	6	
MERRYFIELD GARDENS	13	53	25%	-	3	10	-	-	-	23.10%	10	-	3	7	1	53.80%	1	3	9	23.10%	69.20%	7.70%	-	-	
NELSON ROAD	10	41	24%	-	7	3	-	-	-	100.00%	3	-	1	1	3	30.00%	-	2	8	20.00%	80.00%	-	-	-	
OLD CHURCH LANE	36	89	40%	-	3	32	1	2	10	30.00%	20	2	17	9	3	25.00%	-	23	11	63.90%	30.60%	2.80%	2.80%	1	
OLD FORGE CLOSE	11	22	50%	-	-	11	-	-	9.10%	27.30%	7	-	2	8	1	72.70%	-	5	6	45.50%	54.50%	-	-	-	
OLD LODGE WAY	6	31	19%	-	1	4	1	1	-	80.00%	4	1	1	4	1	66.70%	-	2	4	33.30%	66.70%	-	-	-	
OLDFIELD CLOSE	6	11	55%	-	1	5	-	-	20.00%	40.00%	2	-	1	3	1	50.00%	-	2	1	33.30%	16.70%	2	1	16.70%	
PETERS CLOSE	22	27	81%	-	-	22	-	-	-	86.40%	19	3	10	10	-	45.50%	-	11	11	50.00%	50.00%	-	-	-	
PYNNACLES CLOSE	5	16	31%	-	1	4	-	-	-	100.00%	4	-	2	2	1	40.00%	-	1	3	60.00%	20.00%	-	-	1	
RAINSFORD CLOSE	8	25	32%	-	7	1	-	-	100.00%	-	-	-	1	2	2	25.00%	-	1	4	12.50%	50.00%	25.00%	12.50%	1	
ROBB ROAD	2	48	4%	-	-	2	-	-	-	100.00%	2	-	-	1	1	50.00%	-	-	2	-	100.00%	-	-	-	
ROSEDALE CLOSE	13	24	54%	-	2	11	-	-	-	45.50%	6	-	5	5	3	38.50%	-	5	8	38.50%	61.50%	-	-	-	
RUSPER CLOSE	6	50	12%	-	1	5	-	-	-	80.00%	4	-	3	2	2	23.10%	-	3	2	38.50%	16.70%	1	-	-	
SANDYMOUNT AVENUE	12	55	22%	-	9	3	-	-	-	20.00%	2	5	1	6	5	33.30%	-	50.00%	8	50.00%	16.70%	16.70%	2	2	
SEPTEMBER WAY	32	161	20%	-	18	12	2	1	-	33.30%	11	1	9	18	4	41.70%	-	-	30	66.70%	16.70%	16.70%	1	-	
SHEPHERDS CLOSE	4	27	15%	-	1	3	-	-	-	78.60%	3	7.10%	2	2	2	56.30%	-	2	1	3.10%	93.80%	3.10%	-	-	
SILVERSTONE WAY	41	69	59%	-	25.00%	75.00%	-	-	-	100.00%	3	-	2	2	2	50.00%	-	20	21	48.80%	51.20%	-	-	25.00%	

ROAD NAMES	No. of responses	No. of properties	% reply	Q3: Are you in favour of parking controls being introduced in your road?				Q4: If you answered no or don't know to question 3, if parking controls were introduced in the road next to yours, would you then want your road be included:				Q5: If the majority of your street voted in favour of being included in a CPZ, which of the following time periods would you prefer:					Q6: Which days would you like it to apply:				
				No reply	Yes	No	Don't know	No reply	Yes	No	Don't know	No reply	1- One hour a day (eg 3pm to 4pm as per zone B)	2- One hour during am and one hour during pm (eg. 10am to 11am and 3pm to 4pm as per existing zone H)	3- One hour during am or pm and one hour during the evening.	4- 3.30am to 6.30pm	5- Alternative suggestion, please specify	No reply	Monday to Friday (as existing in zone B)	Monday to Saturday (as existing in zone H)	Monday to Sunday
SPRING LAKE	4	12	33%	-	2	2	-	-	2	100.00%	-	-	-	-	1	-	4	-	-		
STANGATE GARDENS	3	10	30%	-	3	-	-	-	-	100.00%	-	-	-	-	-	3	-	-	-		
STANMORE HILL	27	106	25%	2	5	20	-	2	5	11	2	10.00%	18.50%	55.00%	10.00%	6	18	3	1		
SUNNINGDALE CLOSE	27	46	59%	-	27	-	-	-	1	25	1	-	16	33.30%	3.70%	1	19	6	1		
TALMAN GROVE	35	73	48%	-	3	32	-	1	3	29	2	3.10%	59.30%	3.70%	1	5	28	2	-		
TEMPLE MEAD CLOSE	13	30	43%	-	11	-	2	1	3	7	2	3.70%	77.10%	5.70%	2.90%	1	4	9	-		
THE CHASE	22	48	46%	-	1	19	2	-	2	14	5	7.70%	30.80%	7.70%	1	4	9	-	-		
THE RIDGEWAY	18	48	38%	-	9	9	-	-	9	9	5	4.50%	40.90%	18.20%	4.50%	5	13	1	3		
TRENCHARD CLOSE	1	29	3%	-	1	-	-	-	-	100.00%	-	-	11.10%	27.80%	16.70%	3	4	10	3		
UXBRIDGE ROAD	7	59	12%	1	2	3	1	-	3	3	1	-	14.30%	42.90%	14.30%	2	3	1	1		
VALENCIA ROAD	11	134	8%	-	5	6	-	-	5	5	4	-	18.20%	45.50%	36.40%	4	7	-	-		
WATER GARDENS	3	18	17%	-	2	1	-	-	1	1	-	-	33.30%	33.30%	33.30%	1	1	-	1		
WENTWORTH PLACE	4	5	80%	-	67.00%	33.30%	-	-	4	4	-	-	50.00%	25.00%	25.00%	2	2	-	-		
WILDCROFT GARDENS	3	15	20%	-	-	100.00%	-	-	100.00%	-	-	-	33.30%	33.30%	33.30%	1	1	-	1		
WILLIAM DRIVE	5	17	29%	-	-	5	-	-	5	5	-	-	20.00%	40.00%	40.00%	2	4	-	-		
WINSOMBE WAY	15	33	45%	-	-	15	-	1	2	12	-	6.70%	13.30%	80.00%	2	7	8	-	-		
WOLVERTON ROAD	41	79	52%	-	-	41	-	2	1	38	-	2	29	70.70%	2.40%	1	31	10	-		
WOOD LANE	6	32	19%	1	-	5	-	1	4	4	-	20.00%	16.70%	83.30%	20.00%	3	2	-	1		
WOODSIDE CLOSE	3	6	50%	-	3	-	-	-	-	80.00%	-	-	33.30%	33.30%	33.30%	50.00%	33.30%	2	-		
WYNCHWOOD AVENUE	31	80	39%	-	4	26	1	3	4	18	2	11.10%	35.50%	48.40%	3.20%	11	16	1	3		
WYNCHWOOD CLOSE	14	28	50%	-	1	13	-	-	13	13	-	-	42.90%	35.70%	7.10%	6	8	-	-		
LITTLE COMMON	4	41	10%	-	-	4	-	1	2	2	-	25.00%	75.00%	25.00%	3	1	-	-			
TUDOR WELL CLOSE	7	13	54%	-	2	5	-	-	5	5	-	-	14.30%	14.30%	14.30%	1	4	2	-		
TOTAL	1253	3980	31%	9	280	932	32	44	88	778	54	394	511	185	86	458	639	83	73		
				0.70%	22.30%	74.40%	2.60%	4.60%	9.10%	80.70%	5.60%	31.40%	40.80%	14.80%	6.90%	36.60%	51.00%	6.60%	5.80%		

Note: The total number of properties only relates to the roads that have responded

APPENDIX K

This appendix consists of an ordnance survey map which is not available electronically.

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APPENDIX

address: 1. SUNNINGDALE CLOSE
STANMORE
MIDDX
HA7 3QL

Project Centre Ltd
Saffron Court
14b St Cross Street
London
EC1B 1JD

7 September 2004

Dear Sirs

Possible extension to the existing
Stanmore Town Centre
Controlled Parking Zone (B Zone)

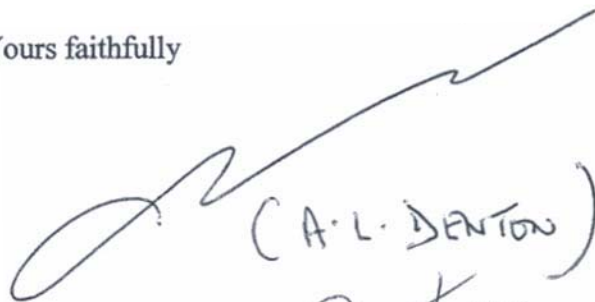
I write with regard to the proposals recently sent me and wish to object in the strongest terms at this idea with respect to my part of Gordon Avenue and the adjacent Sunningdale Close, which is by the way a Cul de Sac.

We do not suffer from commuters, we have no schools, places of religious worship, meeting halls, shops, railways station nor any other possible cause for creating problem that calls for controlled parking.

It would be a completely useless exercise, serving no purpose whatsoever and I protest most fervently at the very idea as I believe do the vast majority of my neighbours and no doubt that you will be hearing from them.

Almost without exception elderly people occupy the homes in our vicinity and one of the most important considerations that would have applied when buying their homes years ago would have been the freedom to park adjacent to their property without hindrance bearing in mind very few have driveways to park in.

Yours faithfully


(A.L. DENTON)
C. Denton
C. DENTON

25 Gleneagles,
Gordon Avenue,
Stanmore,
HA7 3QG.

20th September, 2004

Project Centre Ltd.,
Saffron Court,
14b St Cross Street,
London, EC1B 1JD.

Dear Sirs,

Possible extension to the existing Stanmore Town Centre Controlled Parking B Zone

I am writing with reference to the proposals recently sent to me, and wish to object in the strongest terms at this idea of residential parking/yellow lines in Gleneagles, which is in fact a cul-de-sac. The Gleneagles estate comprises of 35 flats/maisonettes.

At Gleneagles, we do not have the problem of commuter parking, there are no places of religious worship, meeting halls, shops etc., which could give cause for creating problems that require controlled parking.

It would be a completely useless exercise, serving no purpose whatsoever except to hugely increase the income of Harrow Council.

Nearly all the properties in Gleneagles are owned by elderly people, and one of the most important considerations when buying their homes years ago, would have been the freedom to park near their property without hindrance, bearing in mind none have driveways to park in.

There is also the added point, that many of the residents have carers and district nurses calling daily or twice daily on a permanent basis. This means it could cost £3 per day for visitors passes, and according to your absurd ruling each residence can only have 10 books per year. What would happen to the resident who requires medical care daily when the allowance of visitors passes have been used?

With so few parking bays to be allotted, what happens when a resident who has paid for either one or two parking bays, cannot find an available place to park as they are being occupied by either another resident or their visitors?

Please give the matter your consideration and note the problems that this proposal would cause the residents of Gleneagles.

Yours faithfully,

A. Harris (Ms)

Address...10... Conway Close,

Stanmore, Middx,

HA7 3RT

15th September 2004

Project Centre Ltd.,
Saffron Court,
14b St. Cross Street,
London
EC1B 1JD

Dear Sirs,

Possible extension to the existing
Stanmore Town Centre
Controlled Parking Zone (B Zone)

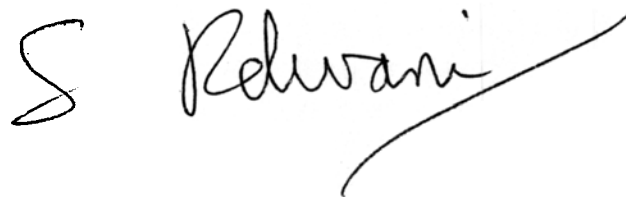
I write with regard to the proposals recently sent to me concerning Controlled Parking, and wish to object in the strongest terms to this idea with respect to Conway Close, which is a Cul-de-Sac.

We do not suffer from commuters, we have no schools, places of religious worship, meeting halls, shops, railway stations nor any other possible cause for creating a problem that calls for controlled parking.

It would be a completely useless exercise, serving no purpose whatsoever and I protest most fervently at the very idea as I believe do the vast majority of my neighbours and I have no doubt you will be hearing from them.

Parking in Conway Close is difficult enough for people who live in the Close, as there is very limited space on the narrow entrance to the Close and most driveways will only take one car which in itself is a problem for people who have two or more cars per household plus, of course, visitors.

Yours faithfully,

A handwritten signature in black ink, consisting of a stylized 'S' followed by the name 'Redwani' in a cursive script, with a long horizontal flourish extending to the right.

Objection to the extension of the Stanmore CPZ

We the undersigned do not accept that there is any sound justification for any extension to the existing CPZ in Stanmore Town Centre (B Zone). We feel that the extent of the present CPZ system is sufficient to meet current needs. We oppose further extension of the current CPZ as this will not improve the traffic and parking where we live.

Name	Signature	Address
A. BINSTOCK		33 THE CHASE
J. BINSTOCK		33 THE CHASE
G. GOODFELLOW		37 THE CHASE
P. GOODFELLOW		37 THE CHASE
A. HILL		43 THE CHASE
M. HILL		43 THE CHASE
D. HILL		43 THE CHASE
N. Paltani		45 THE CHASE
S. Taylor		72 Gordon Avenue
Vivien Waters		51 THE CHASE HA7 3RS
SARINJI DIHANTI		53 THE CHASE HA7 3RS
ZETRA RANTAN		53 THE CHASE HA7 3RS
TARUNA DIHANTI		53 THE CHASE HA7 3RS
HASSAN DIHANTI		53 THE CHASE HA7 3RS
TEBORAH GEXEL CHINERY		15 CONWAY CLOSE HA7 3RT
BILL DAVIS		14 CONWAY CLOSE HA7 3RT
Milly Davn		14 CONWAY CLOSE "
Simon Davis		14 CONWAY CLOSE
Calan Martin		14 CONWAY CLOSE
MARION TAM		12 Conway Close HA7 3RT
David Tan		12, CONWAY CLOSE, HA7 3RT
B. ROSEN		11 CONWAY CLOSE HA7 3RT
N. ROSEN		11 CONWAY CLOSE HA7 3RT
Sanghrakha family		9 CONWAY CLOSE HA7 3RT
P. Jessop family		8 " " HA7 3RT
M. Sanghrakha		9 CONWAY CLOSE HA7 3RT
M. PITTEL		1 CONWAY CLOSE HA7 3RT
TITLOW		1 CONWAY CLOSE HA7 3RT
S. DAVIS		27 The Chase
B. Keary		25 THE CHASE
P. KEARY		25 THE CHASE
K. MCKENZIE		21, THE CHASE.

**Flat 4, Seven Gordon Avenue
Stanmore
Middlesex HA7 3QE**

Tel: 020 8954 7537

Project Centre Ltd
Saffron Court
14b St Cross Street
London
EC1B 1JD

21st September 2004

Dear Sir or Madam

**Possible extension to the existing Stanmore Town Centre Controlled Parking Zone
(B Zone)**

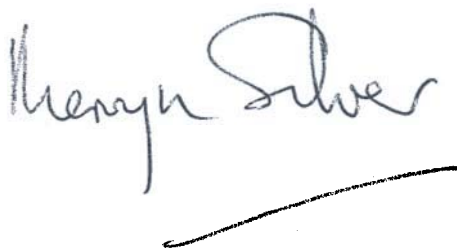
I am writing this letter on behalf of the under-mentioned residents of a block of flats in Gordon Avenue known as Seven Gordon Avenue.

Yesterday evening we held a meeting to discuss the proposals that you circulated, although I must point out that a number of the under-mentioned signatories never actually received the document that your company prepared.

After due consideration, I can confirm that we voted **unanimously against the proposal to implement parking controls within Gordon Avenue**. We did all agree, however, that whatever the outcome, the following should be implemented, as it will improve the sight of vision when exiting from our block and thus greatly reduce the possibility of a serious accident.

- 1 The proposal within the plan to extend the single yellow to the left edge of our garage exit (as you face the block)
- 2 That a similar yellow line should be installed to the right of our garage exit (as you face the block) for about 5metres
- 3 That both of the above yellow lines should be installed on the basis that parking on them is prohibited at all times

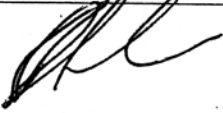

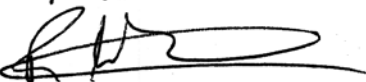


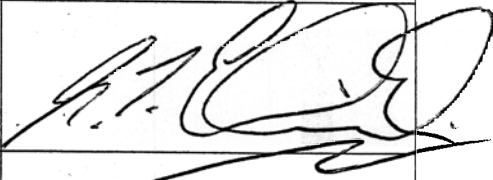
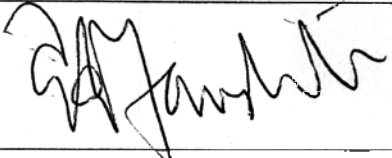
Yours sincerely



Henry Silver

Flat { ^{Dot} E. Solley
 Flat B. Solley H. Spring M. Silver G. Luer
 Flat D. Lee I. Lewis
 Flat Lee
 Flat10 ^{ac} ^{at} [~]
 Flat11 Peter R. Abramo Carlo C. Romo
 Flat12 Lee
 C. de L. J. Lee

WE, THE UNDERSIGNED, CAN SEE NO VALID REASON FOR HARROW COUNCIL'S PLAN TO EXTEND THE EXISTING STANMORE TOWN CENTRE CONTROLLED PARKING ZONE TO OUR PART OF GREEN LANE. WE ARE AGAINST THE PROPOSAL AND OUR REASONS ARE LISTED ON THE SEPARATE SHEET OF PAPER, ATTACHED TO THIS PETITION.

NAME	ADDRESS	SIGNATURE
B. STEELE	2 GREEN LANE COTTS, GREEN LANE, STANMORE HA7 3AE	
P. COLLEY	2 CHART COTTAGE GREEN LANE STANMORE, HA7 3AB	
D. TOBE	4 CHART COTTS GREEN LANE STANMORE HA7 3AB	D. Tobe
G. COOK R. WOOD	2 GREEN LANE COTTAGES GREEN LANE STANMORE HA7 3AE	G & L Cook 
R. STRANGE	1. Green Lane Cottages Green Lane Stanmore HA7 3AE	
T. STRANGE	1 Green Lane Cottages Green Lane, Stanmore HA7 3AE	Toni Strange
W. GREEN.	PEAR TREE COTTAGE. GREEN LANE. HA7 3AB.	
S. MAJUNOWSKI.	PEAR TREE COTTAGE, GREEN LANE HA7 3 AB	
L. Sackman	31, Culverlands close. Green Lane STANMORE	L. Sackman
A FARISHITA	22 CULVERLANDS CLOSE, STANMORE, MDDX HA7 3AG	
F. ROSE	23 CULVERLANDS Close STANMORE MDDX HA7 3AL	F. Rose



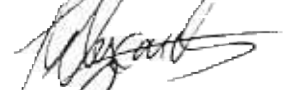

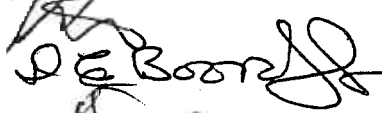


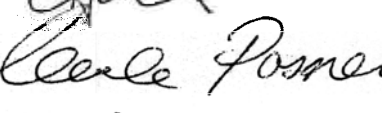
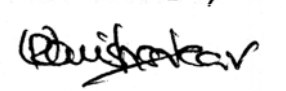
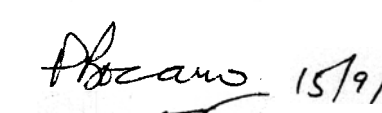
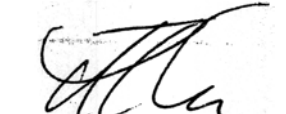
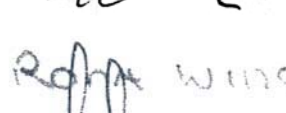


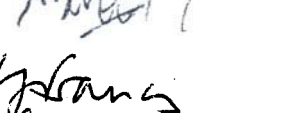
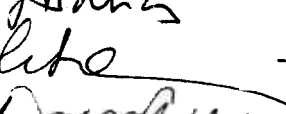


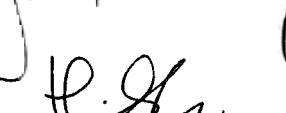
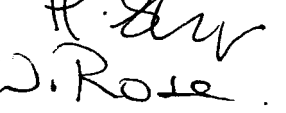
PETITION BY THE RESIDENTS OF GREEN LANE,
 STANMORE TO BE PRESENTED TO HARROW
 COUNCIL, WHO ARE AGAINST THE PROPOSED
 PARKING RESTRICTIONS TO GREEN LANE

Reiss - R

House No./Name	Residents Name	Residents Comments
1) OLDE COTTAGE	A. BAILEY	NO RESTRICTIONS AT ALL
2) Red Tiles	M. FLOWER	NO RESTRICTIONS AT ALL
3) WINDWOOD	A. KEMER	NO RESTRICTIONS AT ALL
4) GREEN LANE COTTAGE	M. SILVER	NO RESTRICTIONS
A) SCHONGE	D. HARRIS	NO RESTRICTIONS
6) Valjean	V. Highman	No added Council - I am
7) Green Corner	C. A. Curtis	No Meters.
8) Helmsdale	R. KIMMEL	No Parking.
9) EVERGREEN LODGE	John	NO PARKING
10) ORANIM	E. ZELKER	NO PARKING - NO METERS
11) Green Wables	G. HOSS	Yellow Lines NO METERS
12) MAURAY	C. KEN	NO PARKING - NO METERS
13) WILLOW LODGE	R. Rubin RUBINI	NO PARKING - NO METERS
14) LUKES PLACE	J. Emanuel J. Emanuel	" "
15) THE STUDIO	VIVIANNE ROSS	" " "
16) 4 CHERCHE-FELLE MEWS	P. FORMAN	" "
17) 17 CHERCHE FELLE MEWS	M. WALKER	NO PARKING - NO METERS
18) 16 CHERCHEFELLE MEWS.	J. L. LORIER	NO PARKING - NO METERS
19) 20 Cherche-felle Mews	P. Lewis	"
20) 8 CHERCHEFELLE MEWS	J. B. B. B.	" " "
21) 6. Cherche-felle Mews	M. P. P. P.	NO PARKING METERS
22) 7 Cherche-felle Mews	J. Seeler	" " "
23) 2 Cherche-felle	J. Goldstein	" " "
24) 1 CHERCHEFELLE	V. White	NO PARKING METERS

Courtens Mews Residents Company Ltd

We, the residents of Courtens Mews, oppose the introduction of parking controls in our road.

House No.	Residents Name	Signature
26	DAVID CEVY	
28.	DAVID DALBER	
19.	PRIT Alexander	
21.	J.H. PERMOTER	
24	D. E. BOOROFF	
	SHIRLEY DURLING	
	A. MAYARAMANI	
	C. POSNER	
	Mrs V Parishankar	
	Phil BOZARRO	
2	Frank deBoa	
4	R Wengate	
20	Sheidan	
17	A B W.	
16	Mr J Francis	
9	Corinne Morris	
10	Claire Donohue	
11	Jean Panny	
	Hayley Shear	
13	Wendy Rose	

The petitioner requests that London Borough of Harrow does not impose in any form whatsoever, controlled parking in Savernake Court and Wolverton Road

NAME	SIGNATURE	ADDRESS	
A. GUERMOUN	<i>[Signature]</i>	7 KENLWORTH HOUSE WOLVERTON RD	HA7 2R
M. P. P. P.	<i>[Signature]</i>	8 KENLWORTH HSE	HA7 2SS
L. G. G.	<i>[Signature]</i>	6 Kenilworth Hse, Harrow	HA7 2SS
MRS J. LOOFLE	<i>[Signature]</i>	3 Kenilworth house	HA7 2SS
A. MCMANUS	<i>[Signature]</i>	2 Kenilworth Hse	HA7 2SS
D. Bloomfield	<i>[Signature]</i>	Jay Cottage Wolverton Rd	HA7 2R
<i>[Signature]</i>	<i>[Signature]</i>	PORTERS COTTAGE,	
F. GRAFTON-HARRIS	<i>[Signature]</i>	ALPHA COTTAGE, WOLVERTON RD	HA7 2R
J. DABBY-JOORY	<i>[Signature]</i>	11 SAVERNAKE COURT, WOLVERTON RD.	HA7 2RA
R. OSTILLY	<i>[Signature]</i>	10 SAVERNAKE COURT, WOLVERTON RD	HA7 2RA
C. OSTILLY	<i>[Signature]</i>	10 Savernake Ct, Wolverton Rd	HA7 2RA
S. LAUBOLT	<i>[Signature]</i>	8 SAVERNAKE CT Wolverton Rd	HA7 2R
I. Booth	<i>[Signature]</i>	2 ^B WOLVERTON RD -	HA7 2RN HA7 2RD
J. E. BOOTH	<i>[Signature]</i>	28 WOLVERTON RD Panmore	HA7 2RN
J. WHEELER	<i>[Signature]</i>	7 WOLVERTON RD Stanmore	HA7 2R
D. WHEELER	<i>[Signature]</i>	7 WOLVERTON RD Stanmore	HA7 2R
M. CUDHAF	<i>[Signature]</i>	11, WOLVERTON RD	
D. CUDHAF	<i>[Signature]</i>	11, WOLVERTON RD	
B. CANTLE	<i>[Signature]</i>	19 WOLVERTON ROAD	HA7 2R
A. J. CANTLE	<i>[Signature]</i>	" " " "	
A. Cape	<i>[Signature]</i>	21 WOLVERTON Rd.	HA7 2R
R. HOWE	<i>[Signature]</i>	23 WOLVERTON ROAD.	" "
J. TINWORTH	<i>[Signature]</i>	29 WOLVERTON RD	
J. Stebbings	<i>[Signature]</i>	31 Wolverton Rd	

Objection to the extension of the Stanmore CPZ

We the undersigned do not accept that there is any sound justification for any extension to the existing CPZ in Stanmore Town Centre (B Zone). We feel that the extent of the present CPZ system is sufficient to meet current needs. We oppose further extension of the current CPZ as this will not improve the traffic and parking where we live.

Name	Signature	Address
M. Johnson		21 Abercorn Rd.
S. Soan		21 Abercorn Rd
A. Gall		26 Abercorn Rd
G. MORRIS		24, ABERCORN RD
J. Lewis		23 Abercorn Rd.
B. E. Byford		22 ABERCORN RD
B. E. Byford		20 ABERCORN RD
M. RUSSELL		18 ABERCORN RD.
B. RUSSELL		18 ABERCORN RD
J. Johnson	J. JOHNSON	17, Abercorn Rd.
L. Martin		16, Abercorn Rd
P. VANDERMOLEN		15 ABERCORN RD
D. VANDERMOLEN		15 ABERCORN RD
B. GILBERT		14 ABERCORN RD
C. GILBERT		14 ABERCORN RD.
L. SHALET		13 ABERCORN RD
A. LUSTIGMAN		10 ABERCORN RD
YASMIN HASAN		7, Abercorn Rd.
E. J. MATHW		6 Abercorn Road
S. E. MATHW		6. Abercorn Road
S. CAWIN		2 Abercorn Road
J. BITT		4 Abercorn Road
B. ISBITT		1, Abercorn Rd
S. ISBITT		1, Abercorn Rd
J. ISBITT		1, Abercorn Rd
G. CHRISTOPHER		87 ABERCORN RD
M. HAZELAN		87 ABERCORN RD
M. HAZELAN		77 abercorn Rd.

please return to 14 Golf Close

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Name	Signature	Address
Toni B Waxman	Toni B Waxman	15 Temple Mead Close
ANGELA SILVERMAN	A. Silverman	17 Temple Mead II
PHILIPPA SILKE	P. Silke	22 Temple Mead Close
TACHI ORAEE	Oran	18 Temple Mead Close
SARITA J PATEL	S. PATEL	18 Temple Mead Close
Sarfraz Bhandal	Bhandal	12 Temple Mead Close
DANN HAUSER	Dann Hauser	9 Temple Mead Close HA7 3RG
Fawzi Lichaa	Fawzi Lichaa	24 Temple Mead Close HA7 3RG
Nadi Lepassi	Nadi Lepassi	28 Temple Mead Close HA7 3RG
ROSENA VARMA	Rosena Varma	30 Temple Mead Close HA7 3RG
JUDITH RICHARD	J. Richard	32 Temple Mead Close
Barbara Freedman	B. Freedman	19 Temple Mead Close
Barbara Freedman	L. BRISTOW	91 Temple Mead Close
Martin Lee	Martin Lee	29 Temple Mead Close
J LE BOW ?	J. Le Bow	42 Temple Mead Close
H. LE BOW ?	H. Le Bow	
R KOTZ	R Kotz	34 Temple Mead Close
Alex Wenduff	Alex Wenduff	36 Temple Mead Close
Al Delmonte	Al Delmonte	13 Temple Mead Close
H. DELMONTE	H. Delmonte	13 Temple Mead Close
E.V. COHEN	E.V. Cohen	20 Temple Mead Close
M. STEEL	M. Steel	26 Temple Mead Close
J. MARTIN	J. Martin	25 Temple Mead Close
E. MARTIN	E. Martin	25 Temple Mead Close
T SELBO	T. Selbo	44 Temple Mead Close

OBJECTION TO THE PROPOSED EXTENTION OF STANMORE CPZ

We, the undersigned, do not accept that there is any sound justification for any extension to the existing CPZ in Stanmore Town Centre

We feel that extent of the present system is sufficient to meet current needs.

We oppose further extension of the current CPZ as this will not improve the traffic flow or safety in the area and will, in fact, lose some much needed parking space in the designated areas.

NAME	ADDRESS	SIGNATURE
M. JOSEPH	1 Colman Court. Gordon Ave	M. Joseph
WALKER	2 Colman Court. Gordon Ave	A Walker
WINTON	3 Colman Court. Gordon Ave	J. Winton
STEINER	4 Colman Court. Gordon Ave	B. Steiner
COHEN	5 Colman Court. Gordon Ave	Cohen
AWAY	6 Colman Court. Gordon Ave	AWAY
ILANTZ	7 Colman Court. Gordon Ave	Ilantz
PEARLMAN	8 Colman Court. Gordon Ave	Pearlman
GELMAN	9 Colman Court. Gordon Ave	Gelman
KATIE	10 Colman Court. Gordon Ave	Katie
LEWIS	11 Colman Court. Gordon Ave	Lewis
HARRIS	13 Colman Court. Gordon Ave	Harris
J. Hoffman	14 Colman Court. Gordon Ave	J. Hoffman
Jones	15 Colman Court. Gordon Ave	Jones
J. E. Schwab	16 Colman Court. Gordon Ave	J. E. Schwab
GERARD GLENN	17 Colman Court. Gordon Ave	Gerard Glenn
Lipson	18 Colman Court. Gordon Ave	Lipson
WIKLER	19 Colman Court. Gordon Ave	Wikler
Michaels	20 Colman Court. Gordon Ave	Michaels
GROVICK	21 Colman Court. Gordon Ave	Grovick
ANGEL	22 Colman Court. Gordon Ave	Angel
BANDEN	23 Colman Court. Gordon Ave	Banden
CAPT. B. LEWIS	24 Colman Court. Gordon Ave	Capt. B. Lewis

**Colman Court
Rosedale Close
Stanmore
HA7 3QF**

2 Greyfeil Close
Stanmore Hill
Stanmore
Middlesex
HA7 3DQ

0208 954 1478

URBAN LIVING TRANSPORTATION SECTION	
2004/28/9	
PASSED	ON 28/9
ACKNOWLEDGED	

For the attention of Owen Northwood
London Borough of Harrow
Civic Centre
HARROW
HA1 2DZ

Copies to: Joyce Markham
Chief Executive
Tony Lear
Executive Director
Urban Living

17th September 2004

Dear Sirs,

Possible extension to existing Parking Zone in Stanmore.

Thank you for the map that was not available for discussion when members of the Greyfell Residents Association attended your "Roadshow" on Saturday 11th September 2004.

It would appear from your map that Harrow Council are under the impression that part of Greyfell Close could have two parking bays imposed upon it, in a way that is discriminatory to some residents and not others.

Residents and Tradesmen delivering or working exclusively on all these properties in The Close have used the parking area behind Nos. 9,10 and 11 since the houses were constructed over 30 years ago. It is after all a cul de sac, suitable only for occupants of these 11 houses, none of which have their own drive-in.

We would point out that according to our deeds, Greyfell Close is a private access road for the "enjoyment" of residents only. In view of this revelation can we be assured that parking in The Close will remain the privilege of residents at no cost, and that it will not be considered a public parking area in the future? We would appreciate your confirmation that there will be a status quo and that this is not a stealth tax imposed by Harrow Council.

The signatories overleaf, who are members of the Greyfell Residents Association, are in full support of these comments.

Yours faithfully



Anthony JE Pike

Members of the Greyfell Close Residents Association

Number One: Mr & Mrs L Davison

L. Davison

Number Two: Mr & Mrs AJE Pike

AJE Pike

Number Three: Mr & Mrs H Benzaken

H. Benzaken

Number Four: Ms A Vaghela

A. Vaghela

Number Five: **MRS. R. VISKINDAS**

R. Viskindas

Number Six: Mr & Mrs J Fixler

J. Fixler

Number Seven: Mrs J Barter

J. Barter

Number Eight: Mr & Mrs J Furmanovsky

J. Furmanovsky

Number Nine: Mrs L McFarland

L. McFarland

Number Ten: Mr & Mrs T Robinson

T. Robinson

Number Eleven: Mr & Mrs K Bazini

K. Bazini

Reference letter from AJE Pike dated 17th September 2004 to Owen Northwood London Borough of Harrow

Objection to the extension of the Stanmore CPZ

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NO CPZ N WYCHWOOD AVENUE

Name	Signature	Address
Amina Dicks		74 WYCHWOOD AVE.
Edwina Neumann	E. Neumann	72 WYCHWOOD AVE.
S. Shah		68 WYCHWOOD AVE
T. Shah		64 WYCHWOOD AVE
M. Wiseman	M. Wiseman	58 WYCHWOOD AVE
M. Russo	M. Russo	56 WYCHWOOD
HARVEY LAMB		70 Wychwood Ave
M. KETT	M. G. Kett	62 WYCHWOOD AVE
B. LATIMORE		52 Wychwood Ave.
N. GOTFCHA		48 WYCHWOOD AVE
A GAUDINO		46 WYCHWOOD AVE
J. Achyng		44 Wychwood Ave
D SHAH		42 WYCHWOOD AVE
N W AHMAD		40 Wychwood Ave
C.I. MARLEN	C.I. Marlen	36 WYCHWOOD AVE.
V. Shah		34 Wychwood Ave
K Sheth		30 Wychwood Ave
N. Renufous		24 Wychwood Ave
DR. M. GONDHIE		22 Wychwood Avenue
G. CARARETTO		16 WYCHWOOD AVE
G. RAO		6 Wychwood Avenue
E. PRICE		26 WYCHWOOD AVE
B. NEWMAN		66 WYCHWOOD AVE
F. KHAN		76 Wychwood Ave.
A. PATEL		38 WYCHWOOD AVE.
D. JACOBS		28 WYCHWOOD AVE
S. Shah.		14 Wychwood Ave.
A. RAVAL		54 Wychwood Ave.
J. AVERSA	J. Aversa	20 Wychwood Ave
M. Patel		18 Wychwood Ave

Objection to the extension of the Stanmore CPZ

We the undersigned do not accept that there is any sound justification for any extension to the existing CPZ in Stanmore Town Centre (B Zone). We feel that the extent of the present CPZ system is sufficient to meet current needs. We oppose further extension of the current CPZ as this will not improve the traffic and parking where we live.

Name	Signature	Address	
BEVERLEY PHILLIPS	B. Phillips	7 Wychwood Close	
A. AHUJA	Ahuja	11 WYCHWOOD CLOSE, HA8 6TE	ETE
S. Ferman	S. Ferman	27 Wychwood Close HA8 6TZ	6TZ
A. PATEL	A. Patel	24 Wychwood Close	
C. SAVASTI	C. Savasti	18 " "	
B. VARIA	B. Varia	6 " "	
G. O'CONNOR	G. O'Connor	8 " "	
P. JANI	P. Jani	19 " "	
C. McDONAGH	C. McDonagh	4 Wychwood Close	
M. G. DUCK	M. G. Duck	2 Wychwood Close HA8 6TE	8 6 TE
JANE MUKURGA	J. Mukurga	1 Wychwood Close HA8 6TE	HA 8 6 TE
ESME BLUSTON	E. Bluston	5 Wychwood Close HA8 6TE	6 TE
Gila Shaw	G. Shaw	25 WYCHWOOD CLOSE, HA8 6TE	6 TE
SEANICE GEORGE	S. George	21 Wychwood Close HA8 6TE	6 TE
COLIN REECS	C. Reecs	21 Wychwood Close HA8 6TE	6 TE
R. Vaghjani	R. Vaghjani	26 Wychwood Close HA8 6TE	8 6 TE
M. Samuel	M. Samuel	23 Wychwood Close	
H. MENDISA-WALTON	H. Mendisa-Walton	22 Wychwood Cl.	
G. MENDISA-WALTON	G. Mendisa-Walton	" "	
Abbey OTULANA	A. Otulana	14 WYCHWOOD CLOSE HA8 6TE	HA 8 6 TE
Balaram Nesbitt	B. Nesbitt	17 " "	
ANGIE REESE	A. Reese	9 WYCHWOOD CLOSE HA8 6TE	HA 8 6 TE
Raphael R. Caputo	R. Caputo	" "	
MARILYN R. CAPUTO	M. Caputo	" "	
JEREMY REESE	J. Reese	9 Wychwood Close HA8 6TE	HA 8 6 TE
Kalindi Shukla	K. Shukla	15 Wychwood Close HA8 6TE	HA 8 6 TE
STEPHEN PHILLIPS	S. Phillips	7 " "	
NEIL PHILLIPS	N. Phillips	7 " "	
H. KEARNEY	H. Kearney	13 " "	
K. SIMON	K. Simon	16 Wychwood Close	
S. Levy	S. Levy	3 Wychwood Close	
Y. Paterson-Jones	Y. Paterson-Jones	12 Wychwood Close	

M. Komar
P.
H.

M. Komar
P. Komar
20 Wychwood Close
28 WYCHWOOD CLOSE

White of Wychwood Close
No to double, no to single lines
159 permits

TR1

26 September 2004

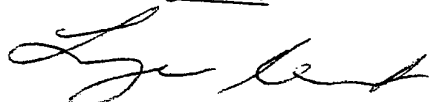




To Harrow Council

Re: Possible extension to the existing
Stanmore Town Centre Controlled Parking Zone

Wentworth Place, Stanmore, HA7 3RN

We, the undersigned, residents of Wentworth Place, object strongly to the possibility of Residents' Parking Permits in our small close.

Wentworth Place consists of only 5 houses, far away from any traffic congestion, with no parking problems.

<u>Name</u>	<u>Address</u>	<u>Signature</u>
SUZANNE + IAN ROBERT	5	
NISHITA + SHAKUNT SHAH	3	
LINDA, PHILIP LYONS	2	
KANJAN, BATUK SHAH	4	
Nicholas & Tomoko Perry		

RECEIVED
30 SEP 2004

URBAN LIVING TRANSPORTATION SECTION	
PA 007 210	
PASSED	ON 3/10
ACKNOWLEDGED	

Objection to the extension of the Stanmore CPZ

We the undersigned do not accept that there is any sound justification for any extension to the existing CPZ in Stanmore Town Centre(B Zone). We feel that the extent of the present CPZ system is sufficient to meet current needs. We oppose further extension of the current CPZ as this will not improve the traffic and parking where we live.

IN PETERS CLOSE WE DO NOT WANT DOUBLE YELLOW LINE OR ANY OTHER RESTRICTION.

Name	Signature	Address
DENISE FELDMAN	[Signature]	3 Peters Close, Stanmore
PETER FELDMAN	[Signature]	3 Peters Close, Stanmore
GERALDINE CUNY	[Signature]	7 Peters Close
R. DESAI	[Signature]	5 Peters Close
L. OSTROFF	[Signature]	11 PETERS CLOSE
R. KINGSEY	[Signature]	15 Peters Close
G. BEAULT	[Signature]	17 Peters Close
M. Kober	[Signature]	21 Peters Close
L. KOBER	[Signature]	21 Peters Close
DAVID WEINGART	[Signature]	23 PETERS CLOSE
Katherine Frieda	[Signature]	35 Peters Close
KELVIN CROUCH	[Signature]	6 Peters Close
SARAH HOPPER	[Signature]	8 Peters Close
JANITA HARRIS	[Signature]	10 PETERS CLOSE
SUSAN BROWNING	[Signature]	16 Peters Close
Rachael Coonan	[Signature]	18 P-6 Close
MARGARET HEEDLER	[Signature]	22 Peters Close
NIGEL GRAY	[Signature]	4 Palm Close
BRAD MARCUS	[Signature]	37 PETERS CLOSE
MANDY MARCUS	[Signature]	37 PETERS CLOSE
HEMANT AMLIWALLA	[Signature]	1 PETERS CLOSE
M. AMLIWALLA	[Signature]	1 PETERS CLOSE
KEENA AMLIWALLA	[Signature]	1 PETERS CLOSE
MR A. PATEL	[Signature]	27 PETERS CLOSE
MRS K. PATEL	[Signature]	27 PETERS CLOSE
MR. A. AMLIWALLA	[Signature]	1 PETERS CLOSE
MR M. AMLIWALLA	[Signature]	1 PETERS CLOSE
MR G. FERRER	[Signature]	20 PETERS CLOSE
MR S. ROBERTS	[Signature]	21 PETERS CLOSE
MRS J. REEDER	[Signature]	24 PETERS CLOSE
Mrs Penn	[Signature]	7 Peters Close
DAVID REARSON	[Signature]	9 Peters Close
DR G. SINGER	[Signature]	19 PETERS CLOSE
MR A. THOMSON	[Signature]	12 PETERS CLOSE
MR S. MEASURES	[Signature]	16 PETERS CLOSE
MR Nash	[Signature]	2 PETERS CLOSE

Objection to the extension of the Stanmore CPZ

We the undersigned do not accept that there is any sound justification for any extension to the existing CPZ in Stanmore Town Centre(B Zone).

We feel that the extent of the present CPZ system is sufficient to meet current needs. We oppose further extension of the current CPZ as this will not improve the traffic and parking where we live.

SS22 Signatures

Name	Signature	Address
MRS C.A. SAKER		3, SILVERSTON WAY
MR R DAVE		16 Silverston Way
M. G. LEVY		40, SILVERSTON WAY
Mr. N. FREEDMAN		38 SILVERSTON WAY
MRS L PHILLIPS		11a SILVERSTON WAY
Mr. Alan Williams		23 SILVERSTON WAY
Mr. Sujan Sadi. N.		25 SILVERSTON WAY
MRS SEWAL		29 Silverston Way
Z.L. Pearson		31, SILVERSTON WAY
MRS S DIAMOND		37 SILVERSTON WAY
MR J CALLAN		39 SILVERSTON WAY
M. S. GREENBERG		41 SILVERSTON WAY
MRS C. INGATE		43 Silverston Way
R LAITOU		5, SILVERSTON WAY
B. SAUNDERS		7 SILVERSTON WAY
M. TABARROK		9 Silverston Way
MR W. COHEN		11 Silverston Way
		1. SILVERSTON WAY
		2 SILVERSTON WAY
		4 SILVERSTON WAY
		6 SILVERSTON WAY
		8 SILVERSTON WAY
		18 SILVERSTON WAY
		20 SILVERSTON WAY
		24 " "
		26 " "
		30 " "
		32 Silverston Way
		32A Silverston Way

Objection to the extension of Stanmore CPZ

We the undersigned do not accept that there is any justification for any extension to the existing CPZ in Stanmore Town Centre (B Zone). We feel that the extent of the present CPZ system is sufficient to meet current needs. We oppose further extension of the current CPZ as we do not have parking or traffic flow problems where we live.

	Name	Signature	Address
1	G. DIAMOND	G. Diamond	5 4000 AND WALK
2	J. GUTWIN	J. Gutwin	9 Holland Close
3	U. BASSO	U. Basso	3 Hollands Close
4	A. CNARES	A. Cnares	7 Holland Close
5	G. TREVIZO	G. Trevizo	1 Holland Close
6	S. ANTOSIC	S. Antosic	6, Holland Close
7	M. MIRANI	M. Mirani	2 Holland Close
8	J. Cohen	J. Cohen	8 Holland Close
9	J. SIMSON	J. Simson	4 Holland Close
10	E. FARR	E. Farr	5 Holland Walk
11	D. GIBB	D. Gibb	8 Holland Walk
12	A. FAUST	A. Faust	9 HOLLAND WALK.
13	C. ZLOTNICK	C. Zlotnick	11 Holland Walk
14	A. COWAN	A. Cowan	11 Holland Walk
15	S. SAIDMAN	S. Saidman	16 Holland Walk
16	BLACKMAN	P. Blackman	18 HOLLAND WALK
17	BENJAMIN	P. Benjamin	6A HOLLAND CLOSE
18	D. HENSON	D. Henson	20 HOLLAND WALK
19	V. DARBIDA	V. Darbida	20 " "
20	J. PHILLIPS	J. Phillips	6 Holland Walk
21	E. ROSE	E. Rose	21 Holland Walk
22	I. HENDERSON	I. Henderson	22 " "
23	D. A. GRANCI	D. A. Granci	23 " "
24	V. J. STRACHAN	V. J. Strachan	1 Holland Walk
25	K. M. STRACHAN	K. M. Strachan	" - " -
26	D. B. STRACHAN	D. B. Strachan	" " "
27	S. DAVIS	S. Davis	3 Holland Walk
28	H. HENLEY	H. Henley	4 Holland Walk
29	S. GUIDA	S. Guida	10 HOLLAND WALK
30	T. FREEMAN	T. Freeman	15 Holland Walk
31	N. MOODALEY	N. Moodaley	17 Holland Walk
32	R. SLATER	R. Slater	19 Holland Walk
33	JOHN ZIEGLER	J. Ziegler	2 HOLLAND WALK

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APPENDIX M

This appendix consists of an ordnance survey map which is not available electronically.

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APPENDIX N

This page consists of an ordnance survey map which is not available electronically.

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APPENDIX O

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APPENDIX P

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TRAFFIC AND ROAD SAFETY ADVISORY PANEL

1 DECEMBER 2004

Chair: * Councillor Miles

Councillors:	* Arnold	* Ismail
	* Branch	* Kara
	* Burchell	* John Nickolay
	* Choudhury	* Anne Whitehead
	* Harriss	

* Denotes Member present

[Note: Councillors David Ashton, Marilyn Ashton, Mrs Bath, Seymour, Silver and Stephenson also attended the meeting.]

PART I - RECOMMENDATIONS**RECOMMENDATION 3 - Stanmore CPZ - Consultation Results**

Your Panel received a report of the Interim Head of Environment and Transport which detailed a review of the existing Controlled Parking Zone (CPZ) in Stanmore and the results of consultation on the proposed extensions to the scheme.

It was advised that the current CPZ was installed 10 years ago in order to address parking problems in Stanmore and the general consensus was that the system was working well. An officer informed Members that the proposed extensions to the CPZ were very limited and the majority of the roads addressed within petitions objecting to the extension of the scheme would not be included on the basis of lack of support. The exception was Howberry Road where a CPZ would be included in the advertised scheme for part of the road.

In the discussion that followed, several Members referred to the recommendations outlined in the officer report and commented that if a number of businesses chose to purchase a permit in order to make use of the parking bays in The Broadway, this would accentuate the existing parking problems already experienced by shoppers in Stanmore. Consequently, the Panel agreed to omit recommendation 2.1g from the officer report and re-number the remaining recommendations accordingly.

Concerning the consultation process, it was stated that a number of the roads included within the proposed extensions were borderline in agreeing to the scheme. As a result the Panel agreed that they be re-consulted. These included Eaton Close, Snaresbrook Drive and London Road. In relation to Eaton Close, it was advised that residents should be re-consulted in parallel with the statutory consultation. With regard to the latter two roads, it was agreed that residents should not only be re-consulted concerning the implementation of the CPZ but also in relation to its times of operation.

Concerning Howberry Road and Howberry Close, Members agreed the recommendation that a CPZ be implemented, but additionally that residents should be written to in order to explain the benefits of the system. Notwithstanding these amendments to the recommendations, a back-benching Ward Member commended the balance of the report and concluded that the roads included were about right.

Resolved to RECOMMEND: (To the Portfolio Holder for Environment and Transport)

That (1) the existing Stanmore Town Centre Controlled Parking Zone B be extended as shown at Appendix M of the officer report;

(2) further consultation be carried out in parallel with statutory consultation in Charlbury Avenue, Craigweil Close and Laburnum Court and if further consultation shows there is no support for inclusion in the CPZ, these roads be excluded from the scheme;

(3) Eaton Road be re-consulted with regard to inclusion in the CPZ, in parallel with the statutory consultation;

(4) further consultation be carried out in parallel with statutory consultation in the proposed zone H extension to include London Road (to Court Drive) and Snaresbrook Drive as shown at Appendix M of the officer report, to establish if there is support for inclusion in the proposed Monday to Saturday extension and if further consultation shows there is no support, these roads be excluded from the proposals;

(5) a Controlled Parking Zone be created in Howberry Road between Cloyster Wood and Wychwood Avenue including Howberry Close as shown at Appendix M of the

officer report to operate, Monday to Friday, 2pm – 3pm and the residents of Howberry Road and Howberry Close be written to in parallel to the statutory consultation in order to explain the benefits of the scheme;

(6) the traffic orders be amended to incorporate the on-street business permit facility for both zones;

(7) the free parking space in Merrion Avenue be converted to 18 shared use “pay and display”/residents/business spaces operating from 8am to 6.30pm, Monday to Friday as shown at Appendix K of the officer report;

(8) double yellow line waiting restrictions be introduced in Stanmore Hill at its junction with Fallowfield, Park Lane, Hilltop Way and Springfield Close as shown at Appendix O of the officer report;

(9) the existing 8am to 6.30pm yellow line waiting restrictions on the south side in Gordon Avenue at its junction with Old Church Lane be extended to the eastern wall of 7 Gordon Avenue as shown at Appendix P of the officer report;

(10) double yellow line waiting restrictions be introduced in Gordon Avenue at its junction with Water Gardens as shown at Appendix P of the officer report;

(11) the existing double yellow line waiting restrictions in Elm Park on the west side be extended northwards to a point opposite the common boundary of 4 and 6 Elm Park as shown at Appendix P of the officer report;

(12) officers be authorised to make minor amendments and finalise the detailed design in accordance with Appendices K, M, N, O and P of the officer report for order making purposes and to take all necessary steps under Sections 6, 45, 46 and 49 of the Road Traffic Regulation Act 1984 to advertise the traffic orders the details of which be delegated to officers and implement the scheme subject to consideration of objections, the statement of reasons to be “to control parking”; and

(13) inform the head petitioners accordingly.

[REASON: To gain agreement for the way forward with a view to implementation of parking controls to address the Council’s stated priority of enhancing the environment and encouraging more sustainable transport activity.]